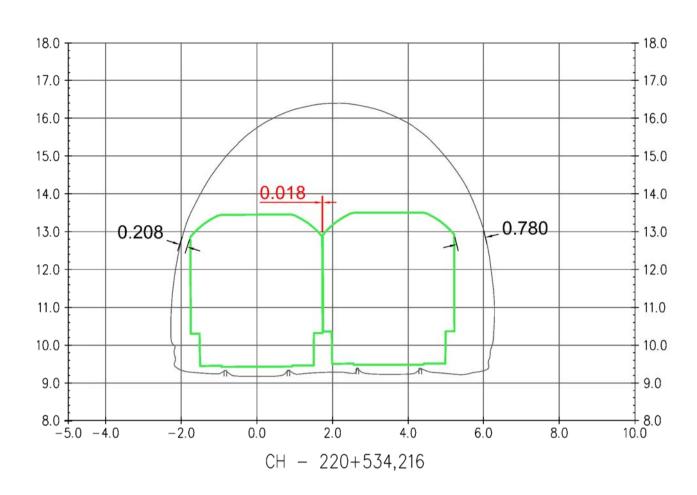
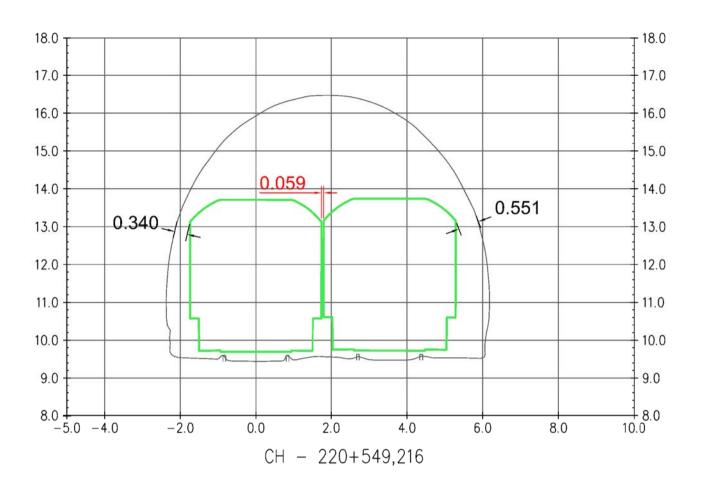
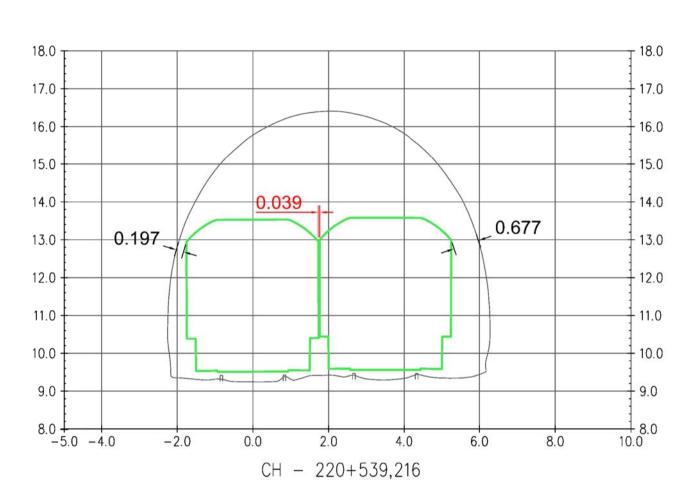
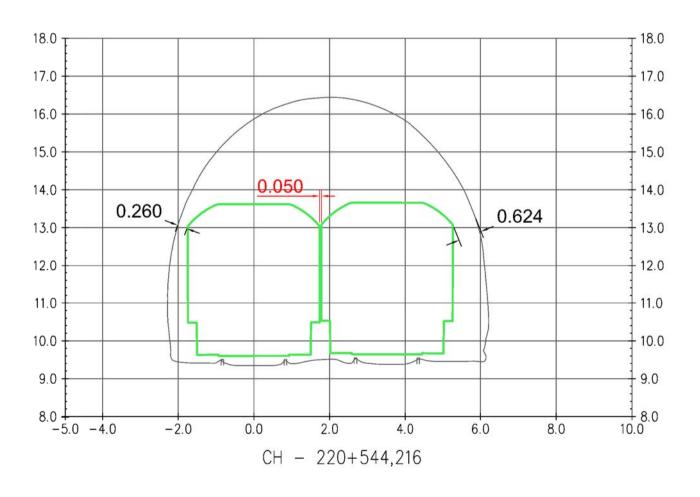
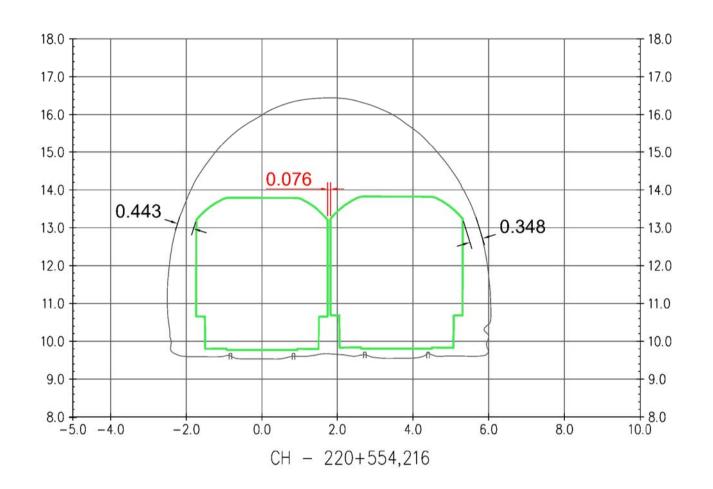
SECTION BETWEEN 220+489,216/220+644,216 (2)

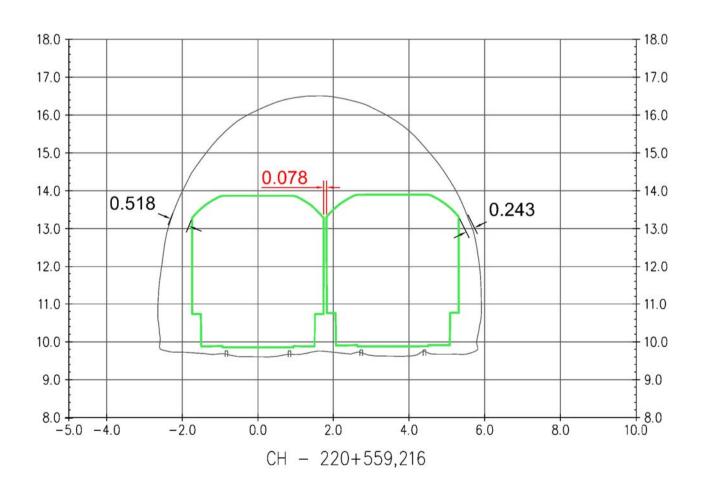


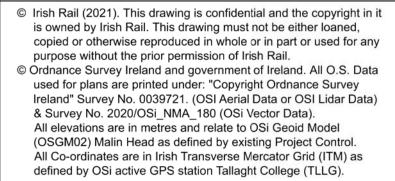




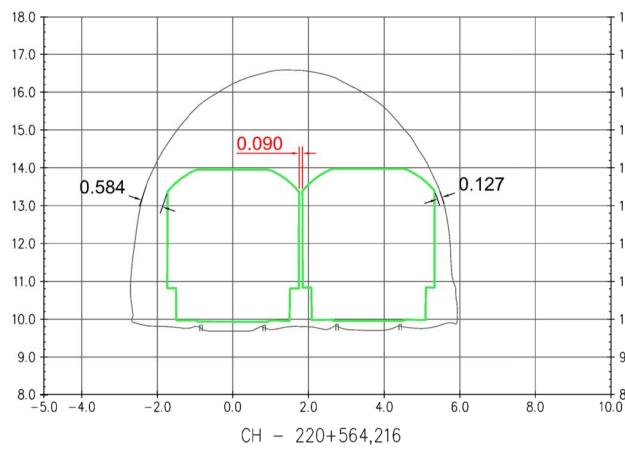


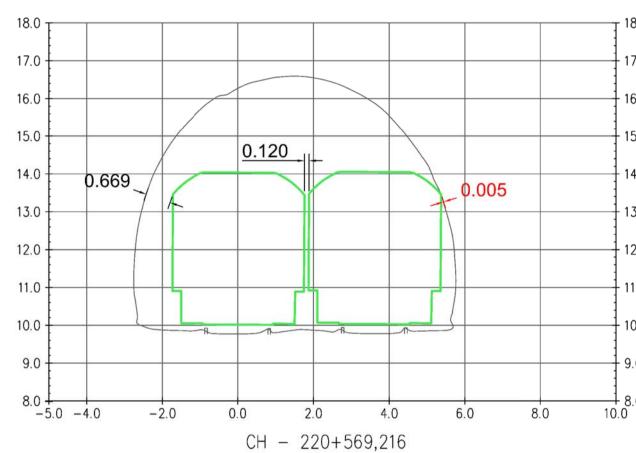


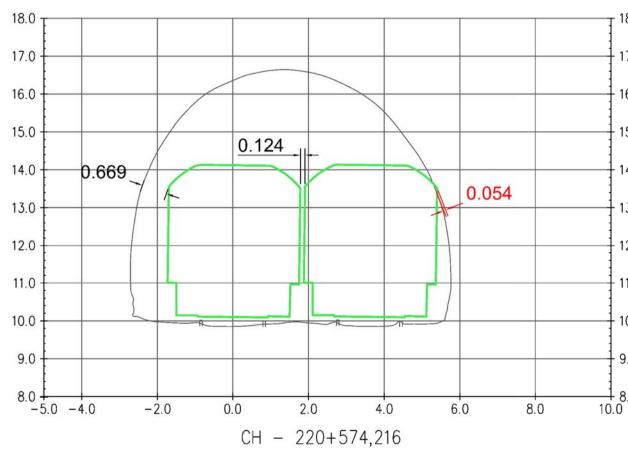










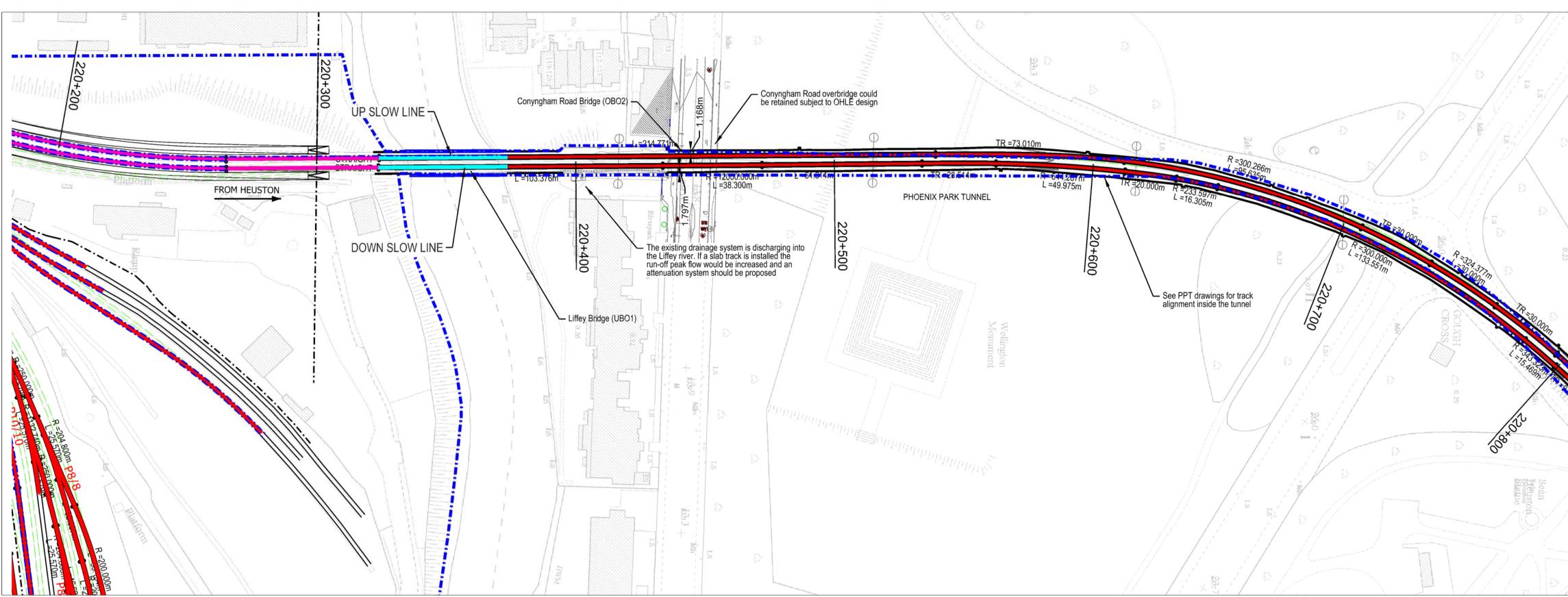


		Rev	Date	Drn	Chk'd	App'd	Description	Client		Engineering D	esigner	
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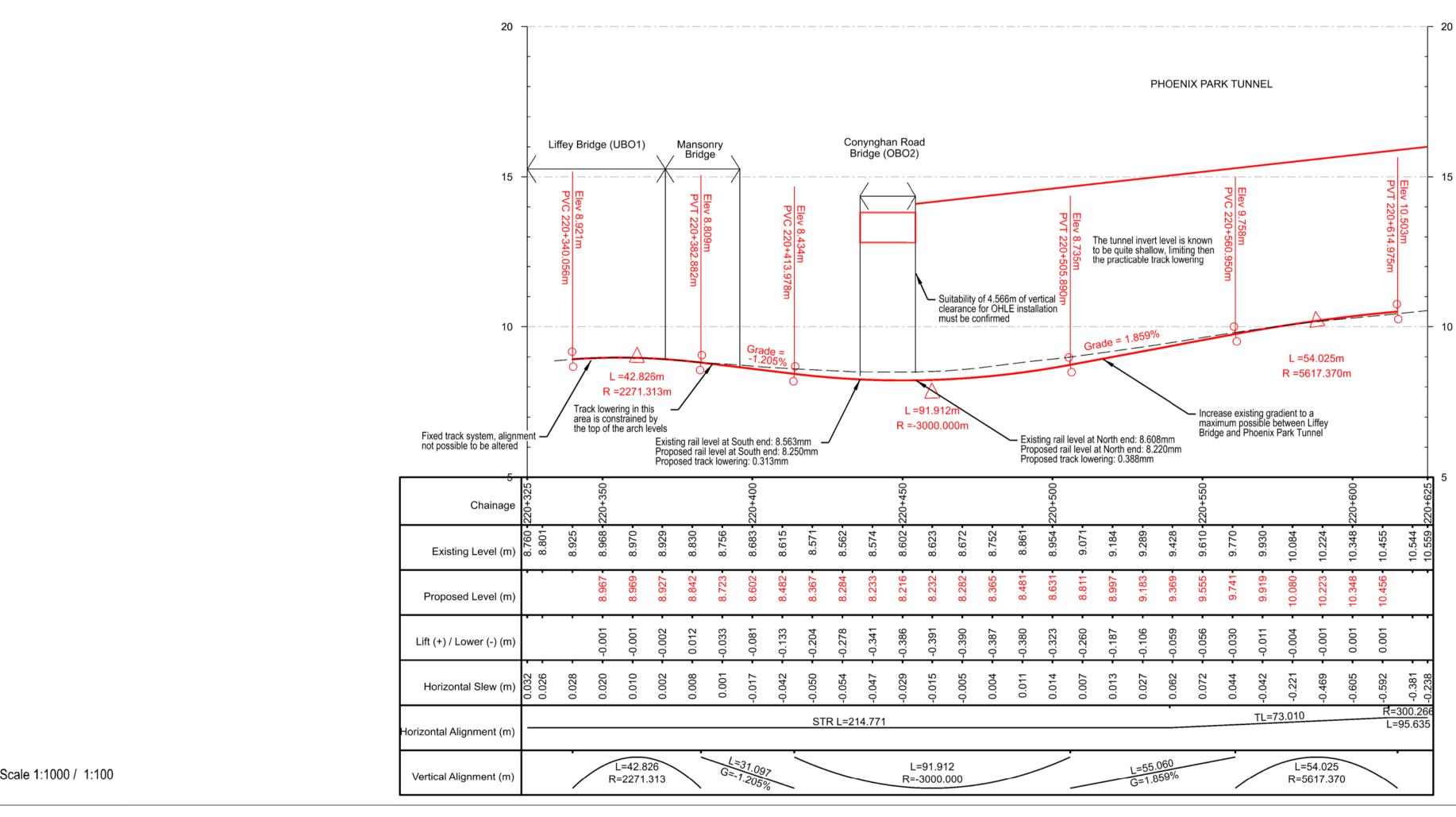
	NOTE:
	In Option 0 a gauging assessment of the existing track is demonstrated Existing track is ballasted
	All distances in metres unless otherwise stated
	IRL2 reference profile (Standard CME-TMS-327 Vehicle Gauging) is used to assess the gauge in existing structures
	Track maintenence tolerances for ballast track are added to the reference profile (Standard I-PWY-1101 Requirements for Track and Structures Clearances): - Track position: 25 mm
	- Gauge widening: 8 mm
	- Crosslevel error: 20 mm - Vertical alignment variation: 20 mm
8.0	Clearance between Reference Profile (+ Track Maintenance Tolerance) and railway operational structure to be 150mm as a minimum. It is assumed no windows for staff or passengers to lean out.
7.0	for staff or passengers to lean out. Vehicle passing clearance between References Profiles (+ Track Maintenance Tolerance) to be 100mm as a minimum. It is assumed no windows for staff or passengers to lean out.
6.0	or passengers to lean out.
	IRL2 Reference Profile + Track Maintenance Tolerances
15.0	
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9.0	NOTE:
3.0	INFORMATION IS PRESENTED FOR PUBLIC CONSULTATION NO. 1.
	NOT ALL DESIGN DETAIL OR CONSTRUCTION RELATED DETAIL ARE KNOWN AT THIS TIME, E.G. SUBSTATION
	LOCATIONS, CONSTRUCTION COMPOUNDS, TEMPORARY BRIDGE REQUIREMENTS, TEMPORARY UTILITY DIVERSIONS AND OTHER ELEMENTS
	POTENTIAL INTERFERENCE WITH PROPERTY RIGHTS IS BASED ON THE LEVEL OF INFORMATION AND DESIGN
	AVAILABLE AT THIS TIME. FURTHER WORK INCLUDING DETAILED DESIGN AND TECHNICAL AND CONSTRUCTION RELATED SOLUTIONS WILL SEEK TO MINIMISE
	POTENTIAL INTERFERENCE WITH PROPERTY RIGHTS
Project Title	DART + SOUTH WEST
P	HOENIX PARK TUNNEL
Approved CROS	SS SECIONS (SHEET 2 of 2) OPTION 0
C AG Drawing File Name	23-DWG-PW-TTA-35757 Version Status

DP-04-23-DWG-PW-TTA-35757

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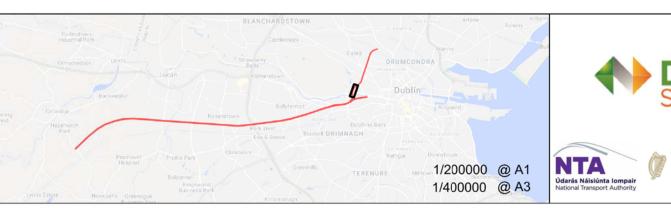


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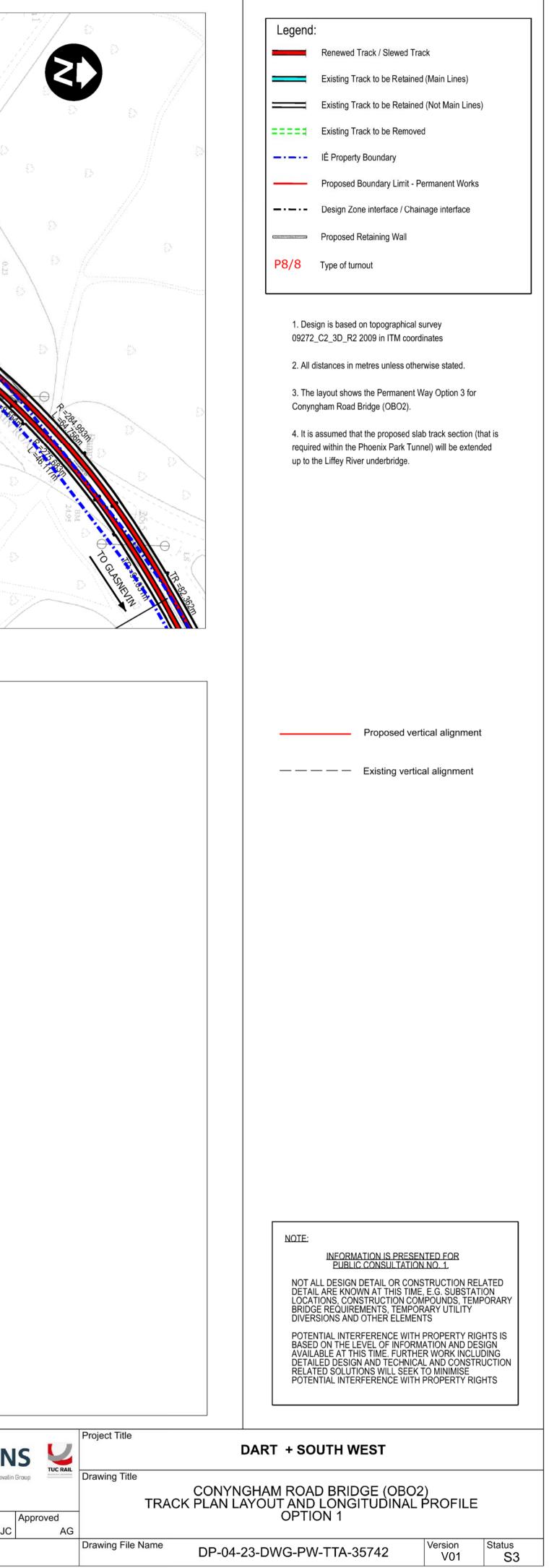


Profile Scale 1:1000 / 1:100

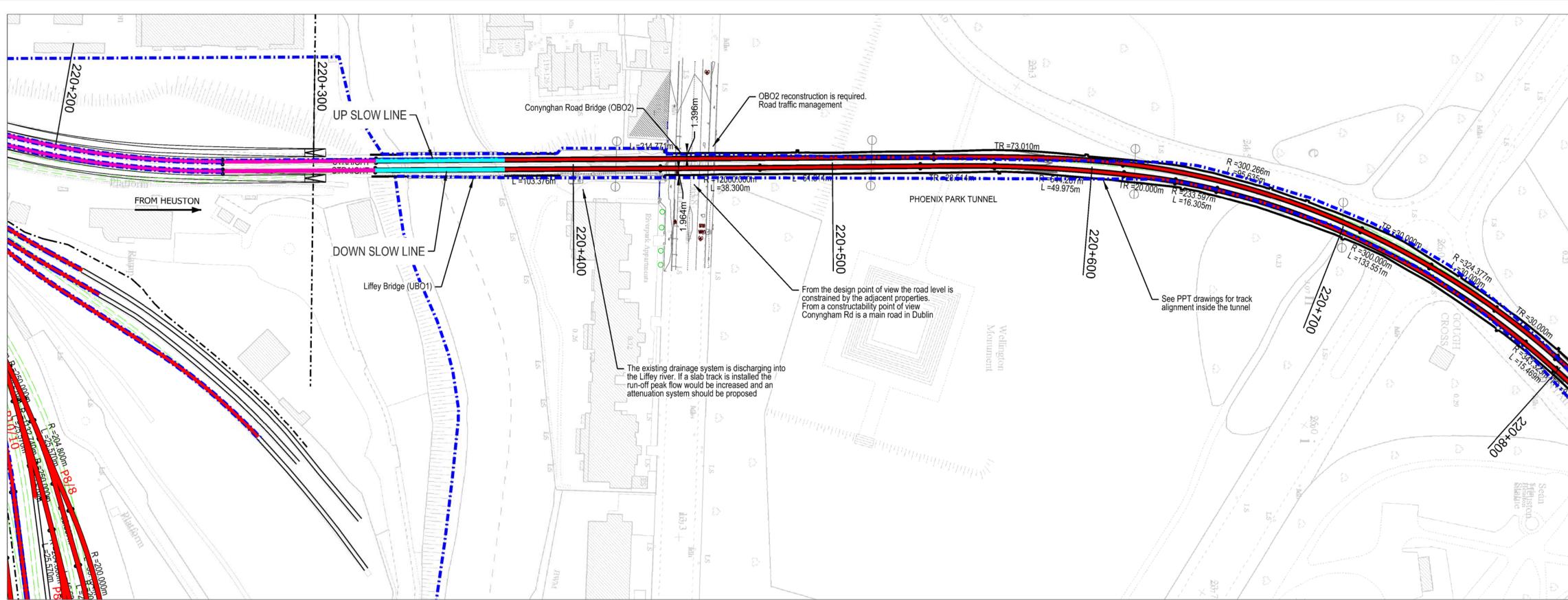
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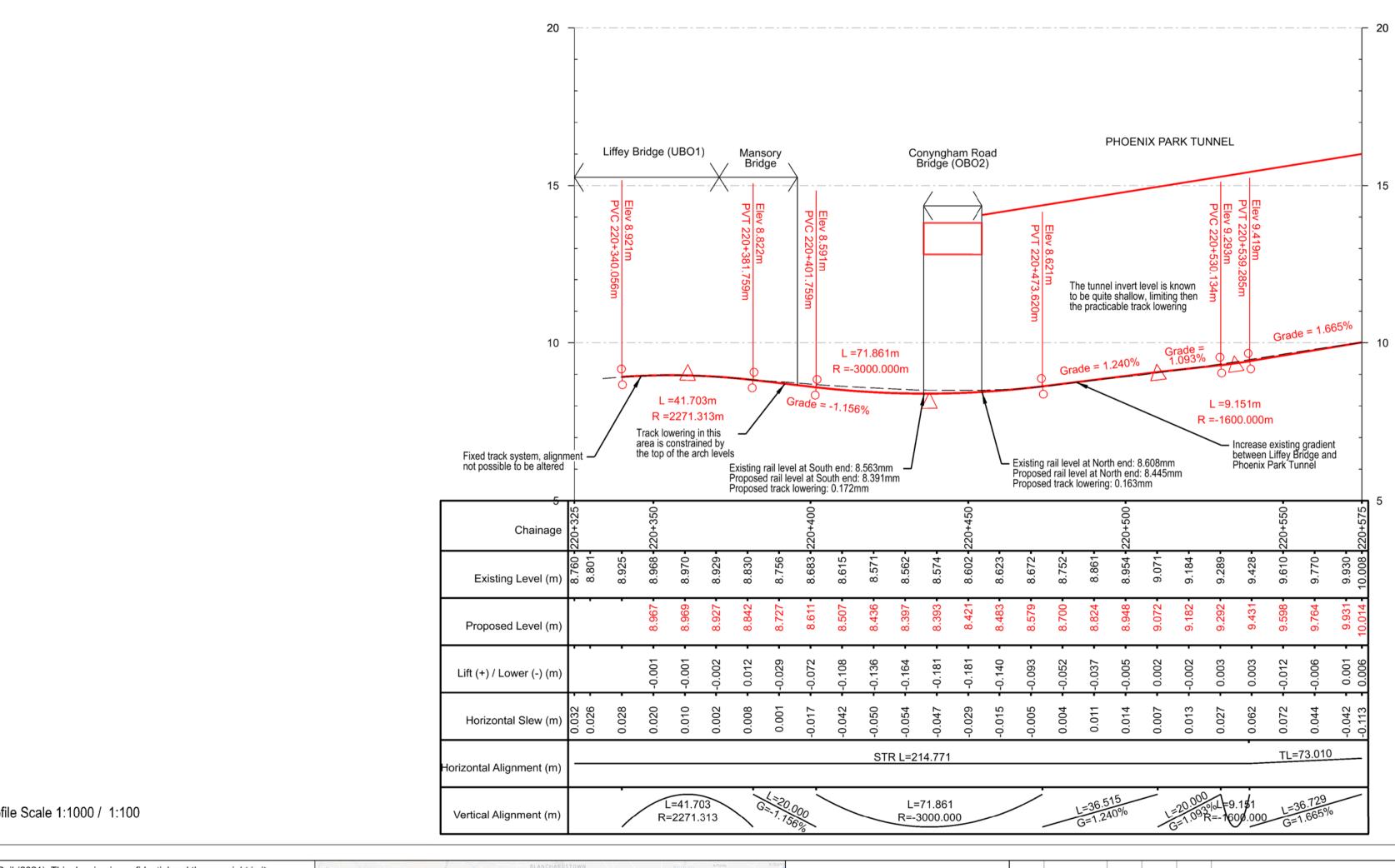
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Plan Scale 1:1000

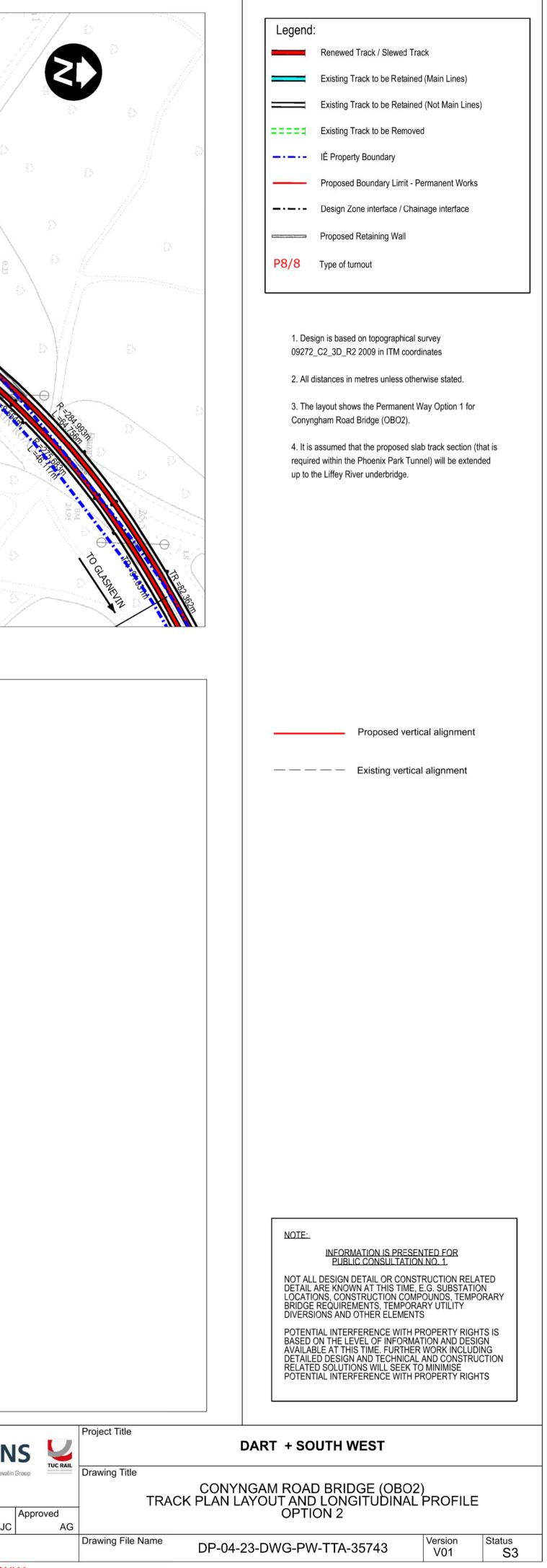


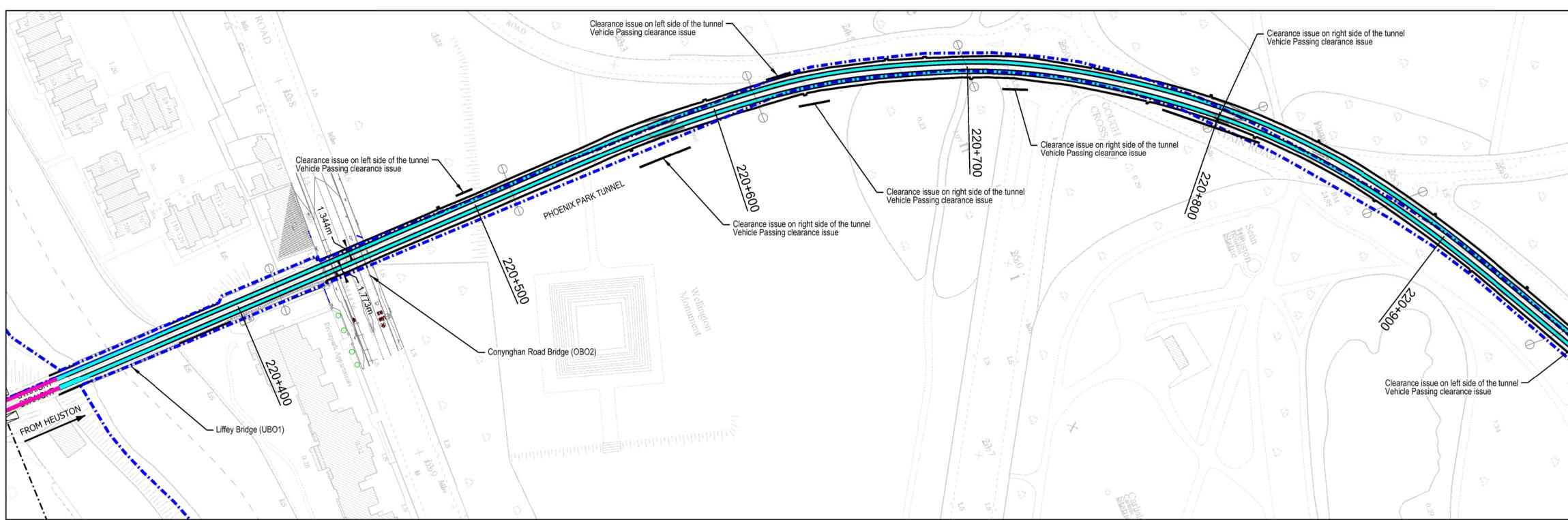
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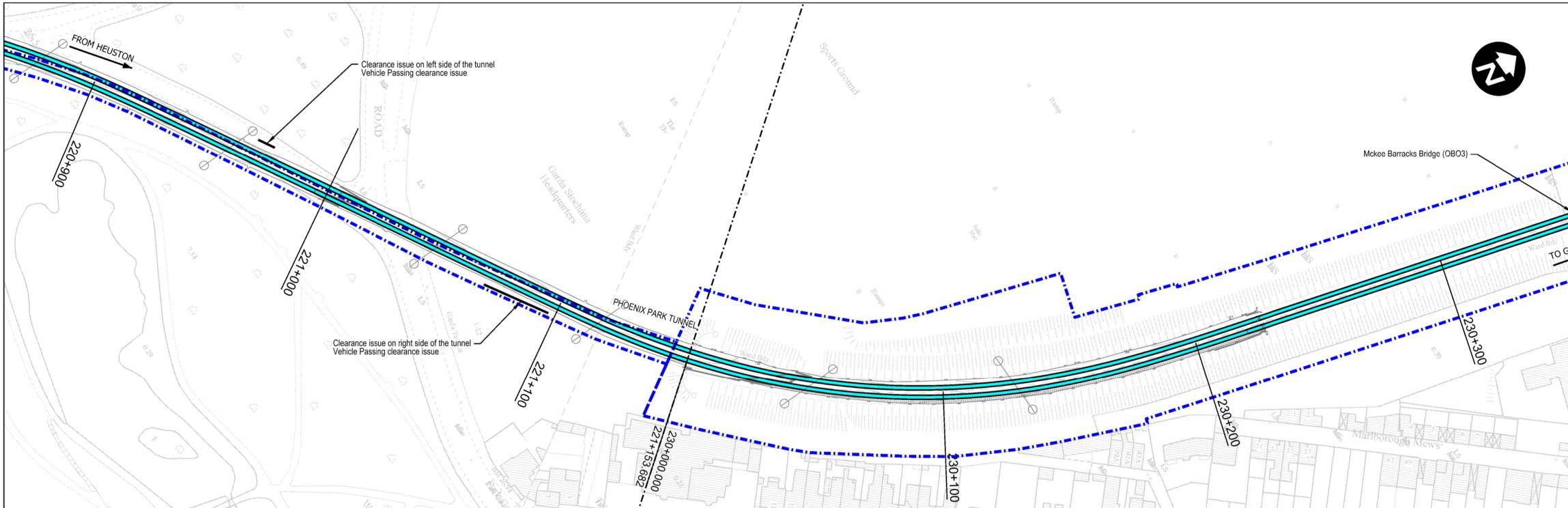


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Plan Scale 1:1000



Plan Scale 1:1000

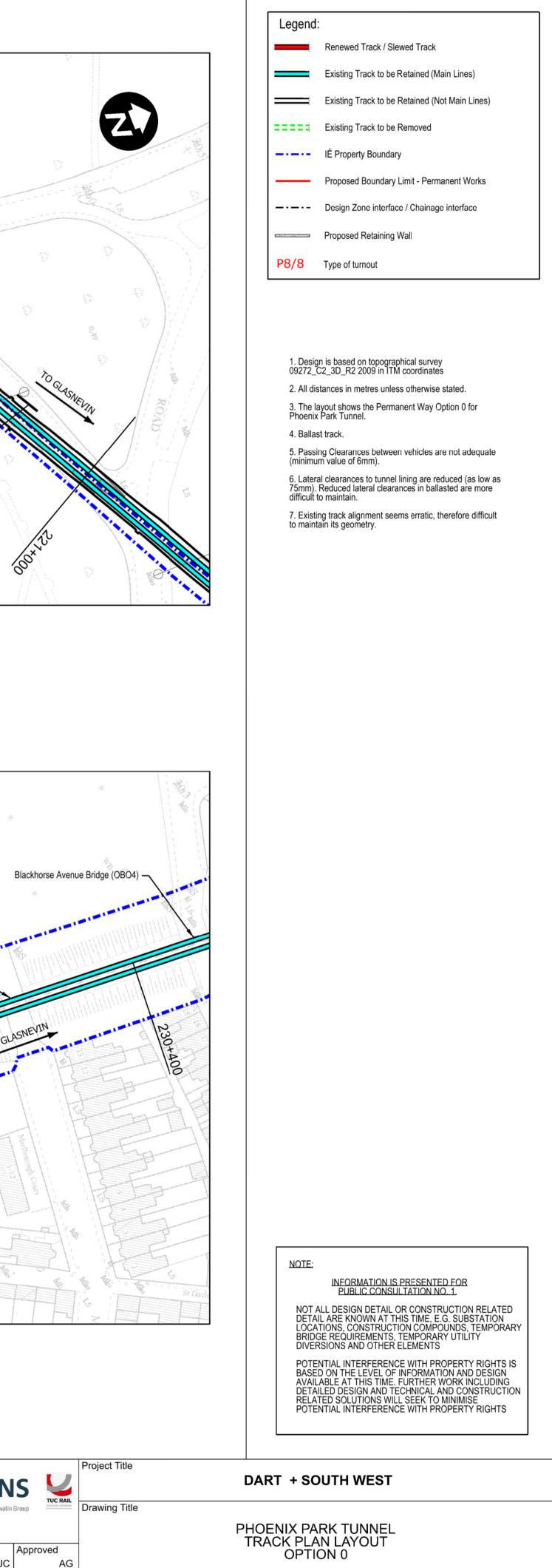
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		Rev	Date	Drn	Chk'd	App'd	Description	Client		Engineering D	esigner
DAR outh V								A larn	ród Éireann <mark>Rail</mark>	TYPSA Me	ATKIN ember of the SNC-Laval
Tionscadal Éireann Project Ireland	200							Date	Scale @ A1 @ A3	Drawn JYM	Checked FJC
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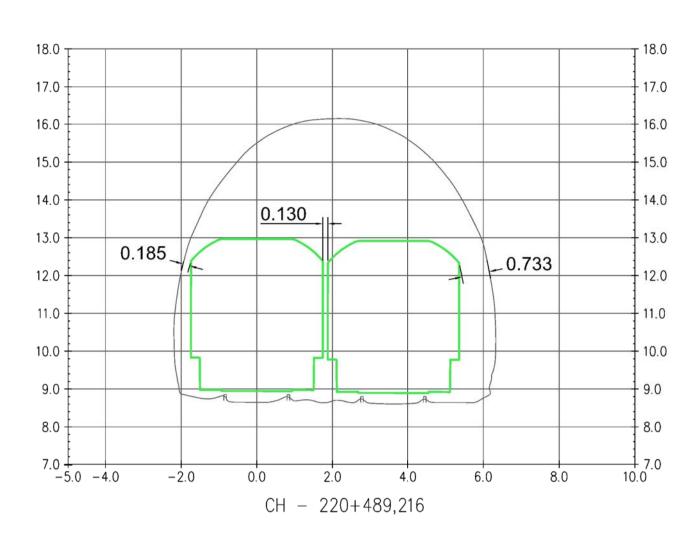
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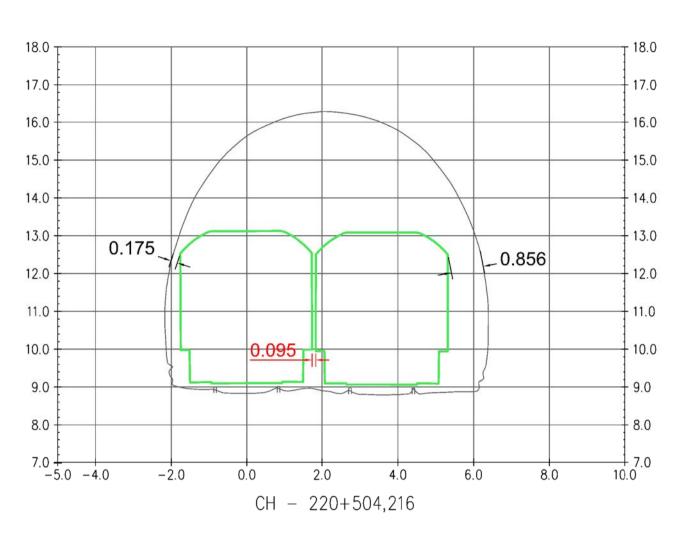
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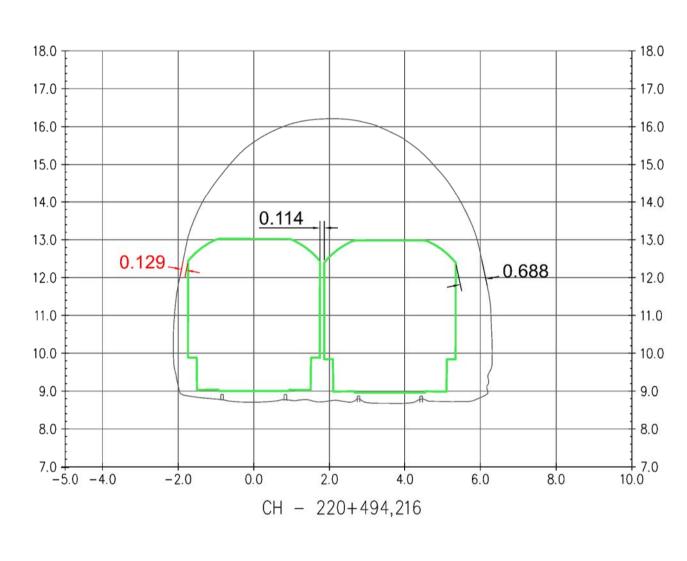
Version Status V01

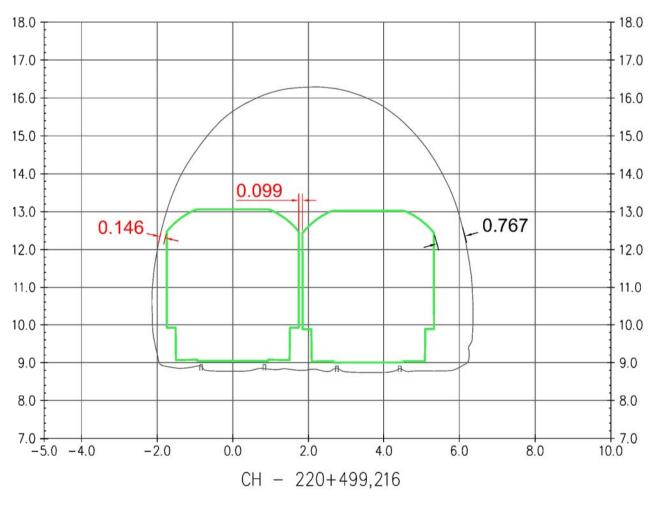
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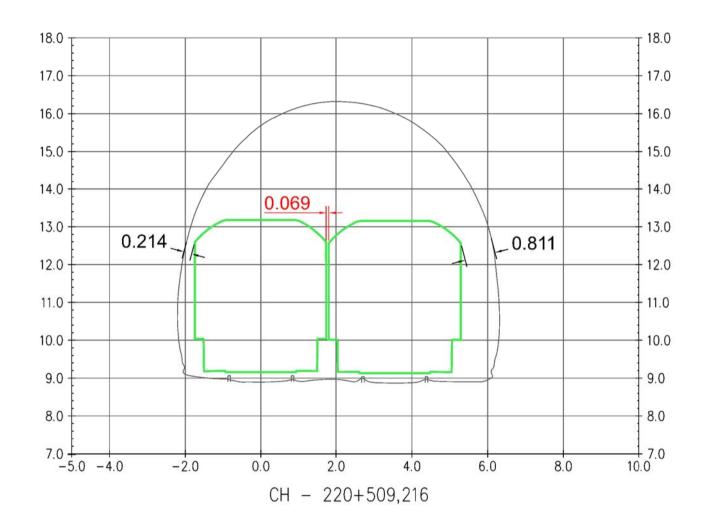


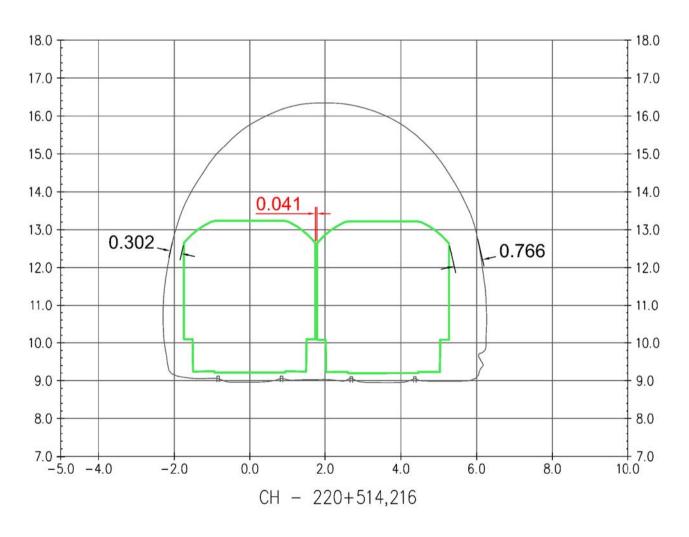




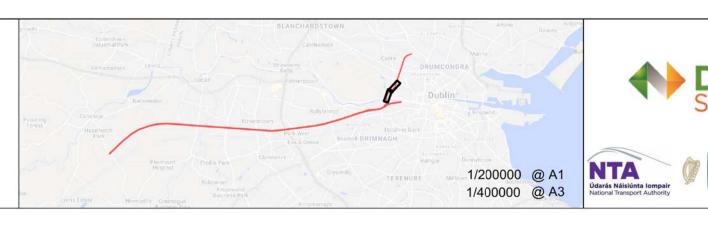




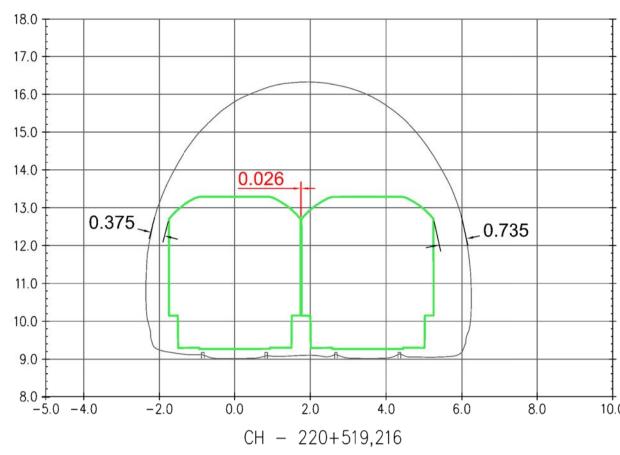


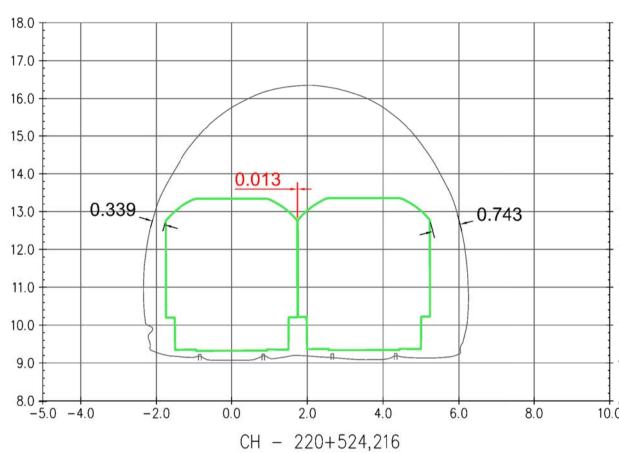


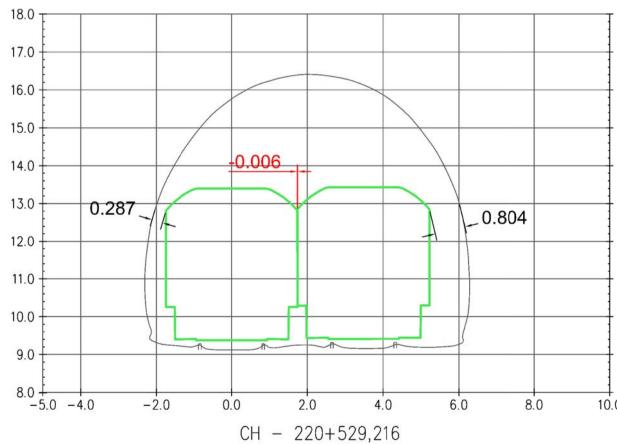
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SECTION BETWEEN 220+489,216/220+644,216 (1)

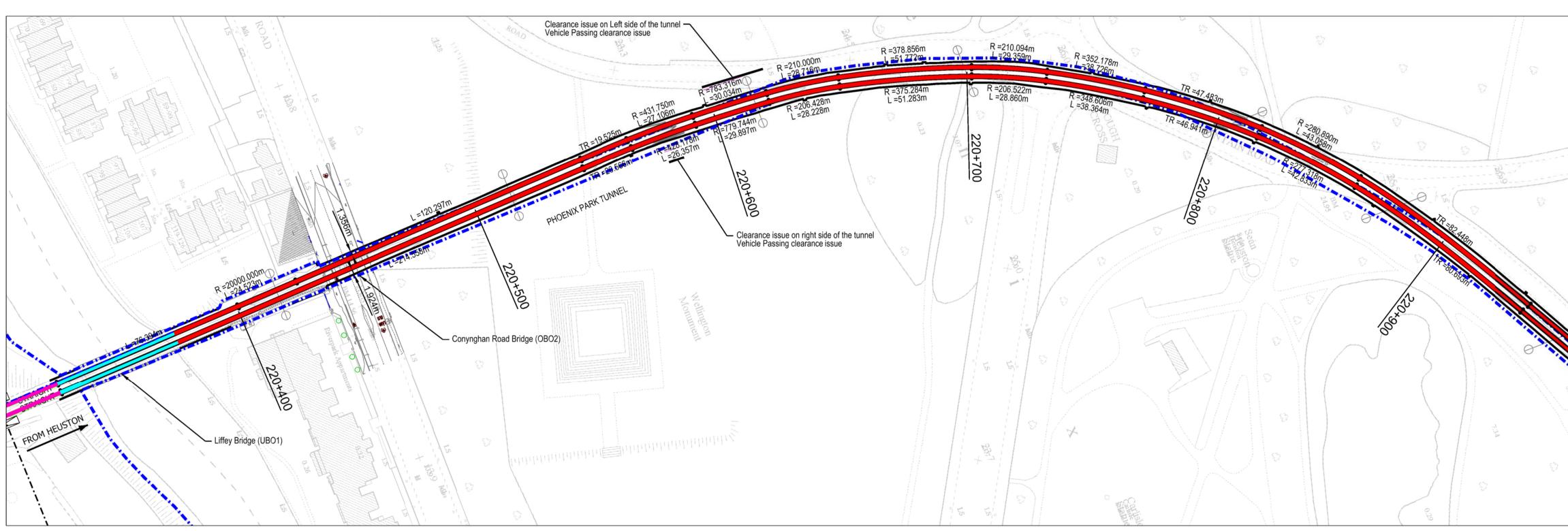




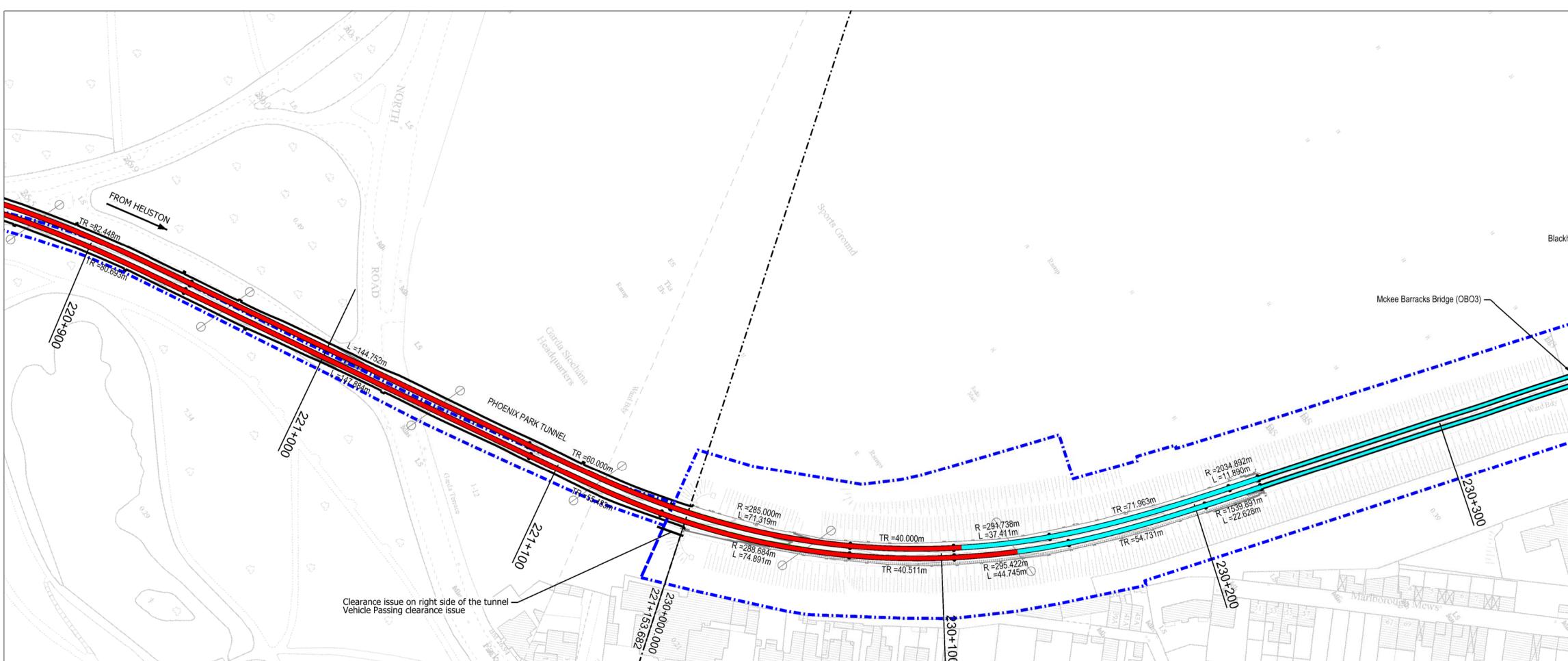


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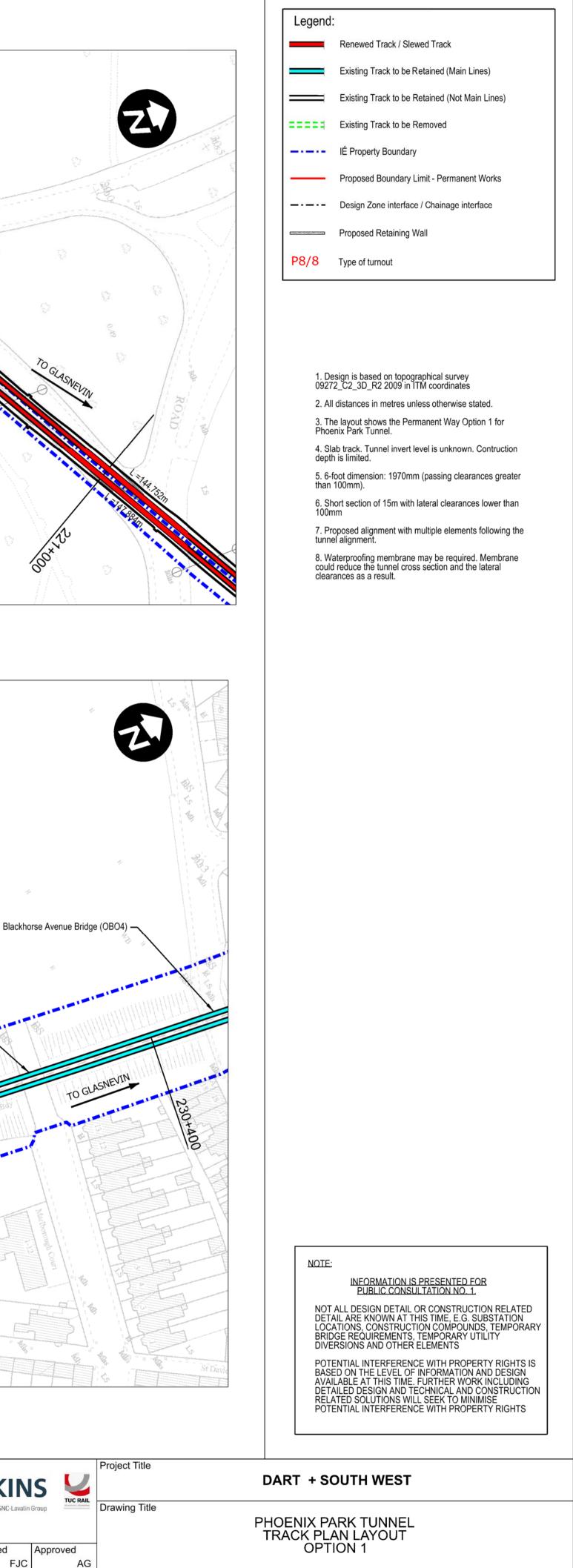


Plan Scale 1:1000



Plan Scale 1:1000

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2040	** **							Project Code	Issuer		QMS Code	•
	Co-financed by II in Connecting Europe Facility of the Ear opeen Union	v01	21/04/2021	JYM	FJC	AG	FOR PUBLIC CONSULTATION No1		ATH	:		



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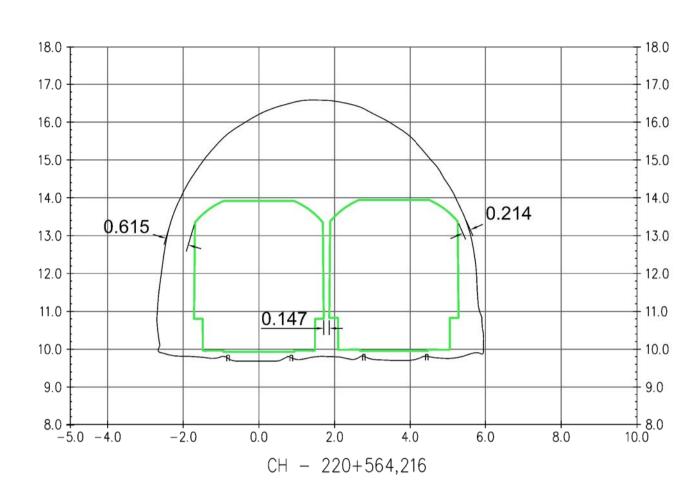
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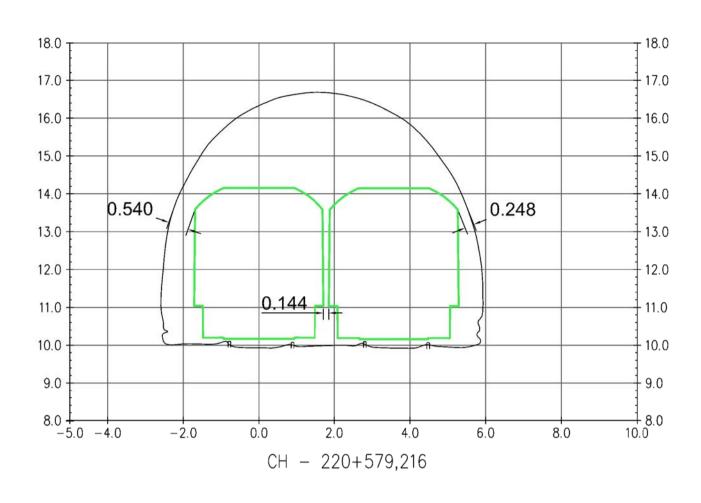
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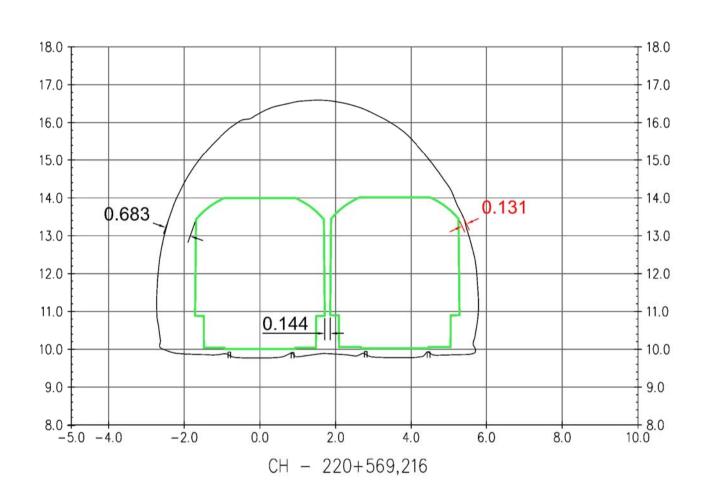
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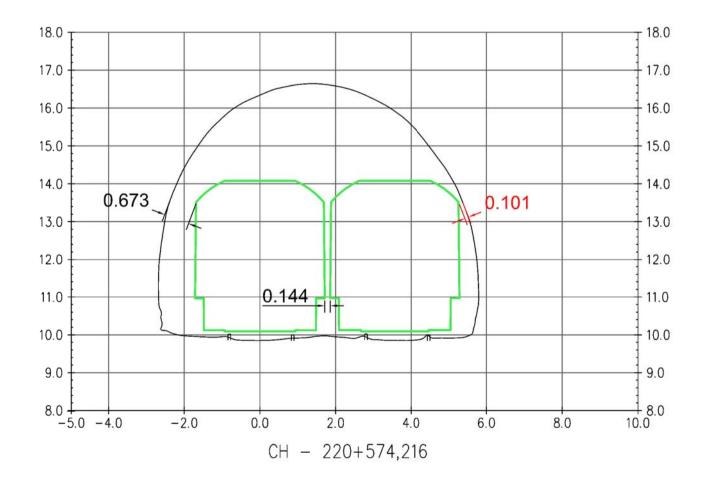
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SECTION BETWEEN 220+564,216/220+579,216

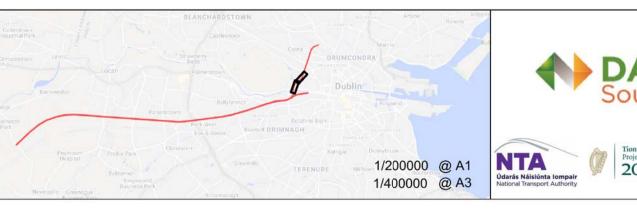








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	Rev	Date	Drn	Chk'd	App'd	Description	Client		Engineering D	Designer
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2040 ****	-						Project Code	Issuer	QMS Code	
Co-financed by the Connecting Europe Facility of the Europeen Union	v01	21/04/2021	JYM	FJC	AG	FOR PUBLIC CONSULTATION No1		ATK		
	22		<u> </u>	372	N					

NOTE	

In Option 1 an optimised track alignment is proposed to comply with IÉ standards. Slab track is proposed in Option 1

All distances in metres unless otherwise stated

IRL2 reference profile (Standard CME-TMS-327 Vehicle Gauging)

- Coefficients of the slab track tolerances adopted for the kinematic gauge (Standard EN 15273-3. Gauges-Part 3: Structure Gauges):
- Track position:0,005 mm
- Gauge widening:0,008 mm
 Crosslevel error:0,005 mm

- Vertical alignment variation:0,005 mm

Clearance between Reference Profile (+ Track Maintenance Tolerance) and railway operational structure to be 150mm as a minimum. It is assumed no windows for staff or passengers to lean out.

Vehicle passing clearance between References Profiles (+ Track Maintenance Tolerance) to be 100mm as a minimum. It is assumed no windows for staff or passengers to lean out.

IRL2 Reference Profile + Track Maintenance Tolerances

NOTE:

INFORMATION IS PRESENTED FOR PUBLIC CONSULTATION NO. 1.

NOT ALL DESIGN DETAIL OR CONSTRUCTION RELATED DETAIL ARE KNOWN AT THIS TIME, E.G. SUBSTATION LOCATIONS, CONSTRUCTION COMPOUNDS, TEMPORARY BRIDGE REQUIREMENTS, TEMPORARY UTILITY DIVERSIONS AND OTHER ELEMENTS

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Approved

AG

Project Title

DART + SOUTH WEST

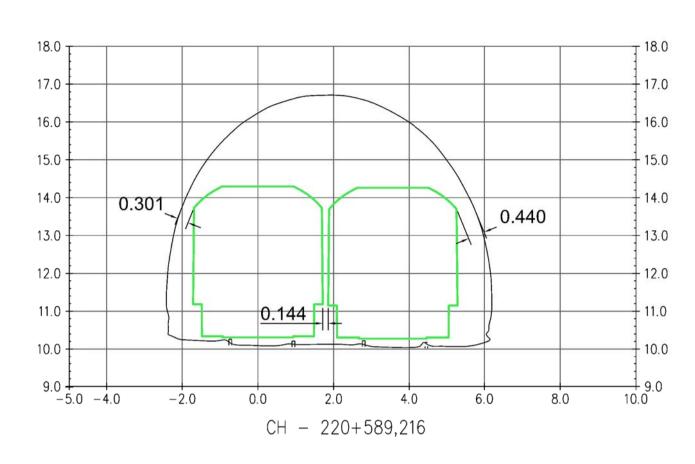
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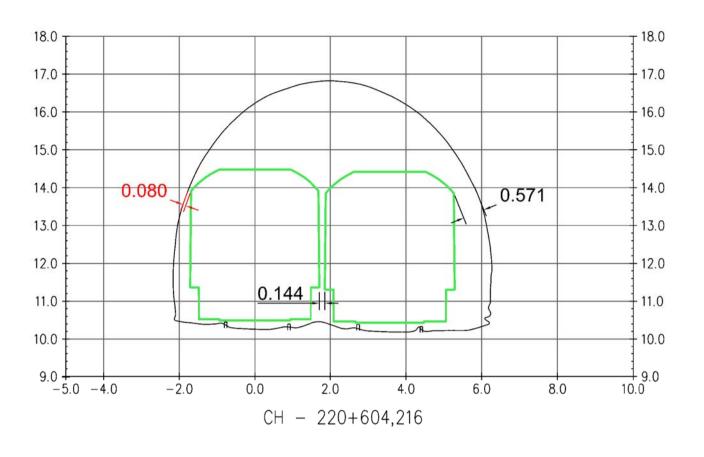


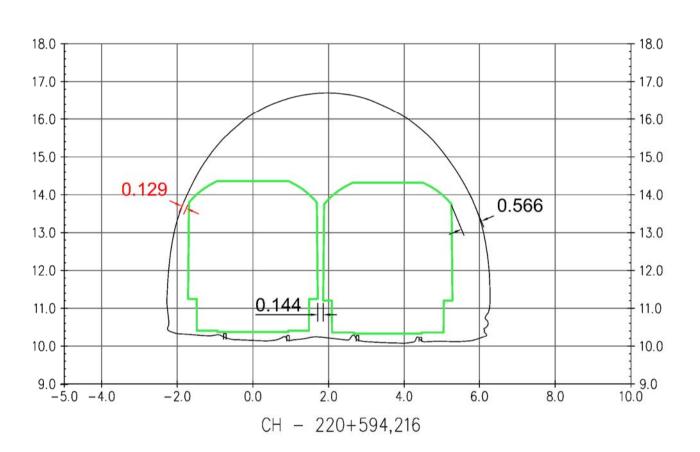
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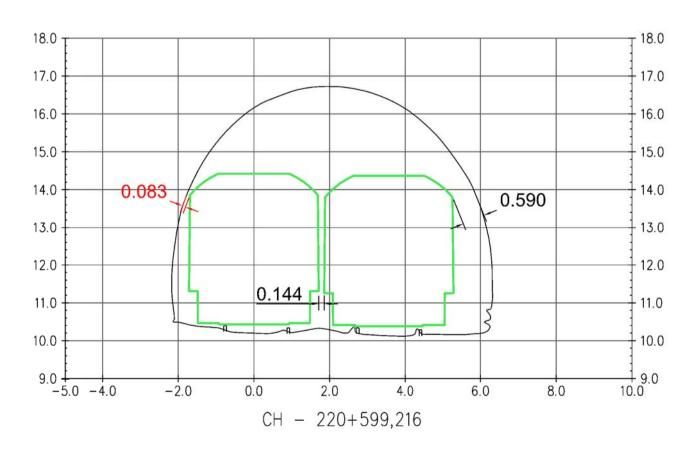
DO NOT SCALE USE FIGURED DIMENSIONS ONLY

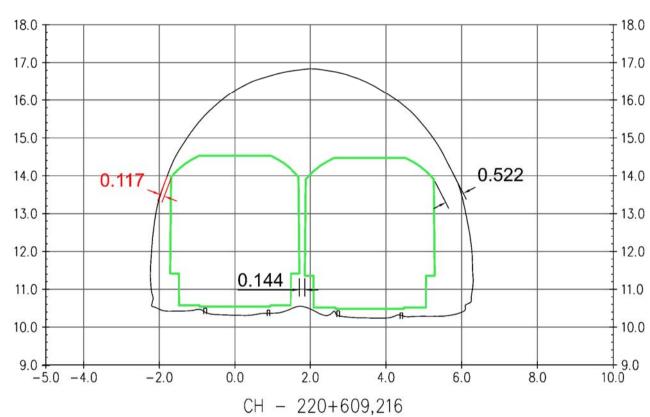
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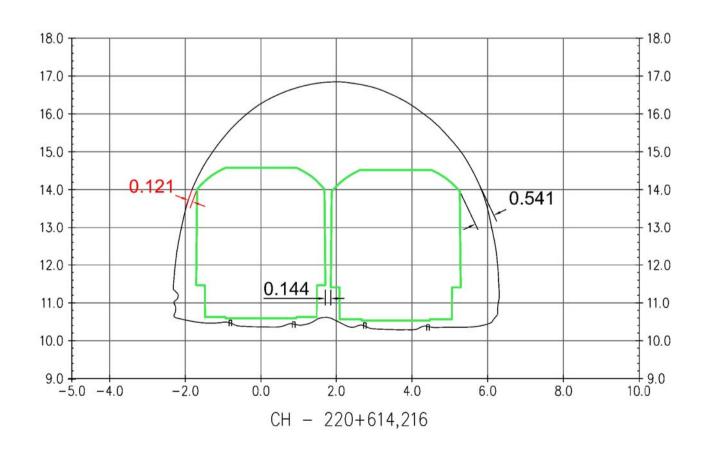




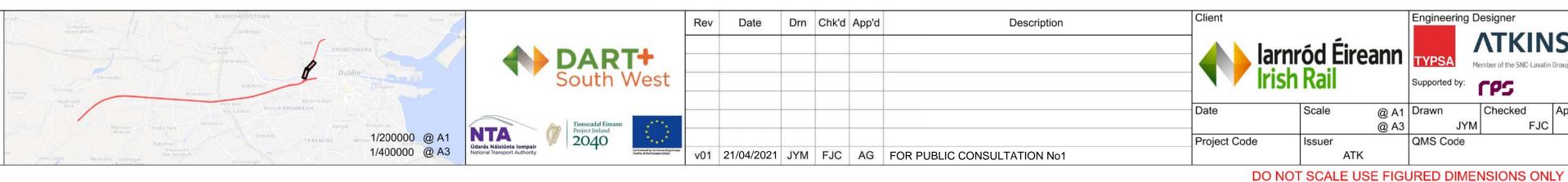




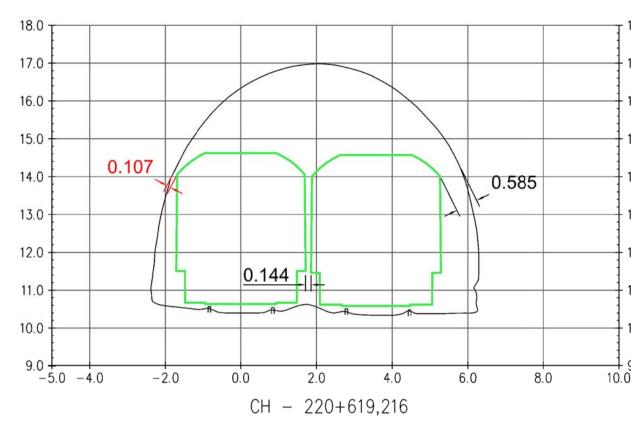




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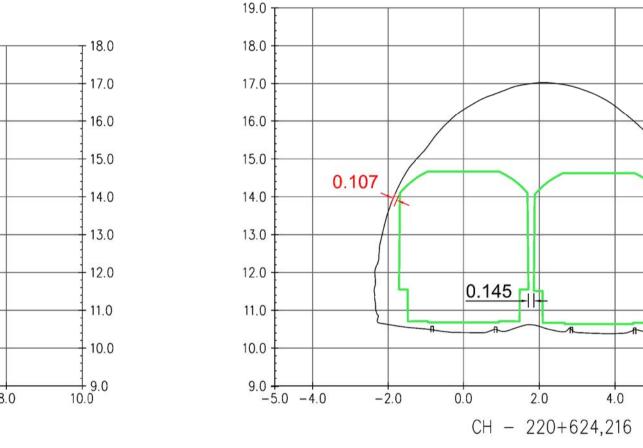
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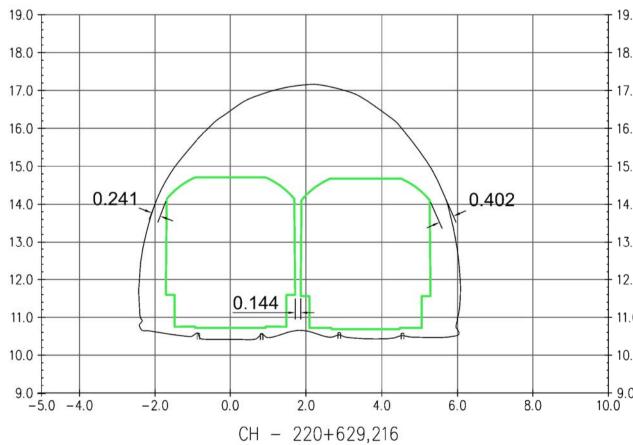


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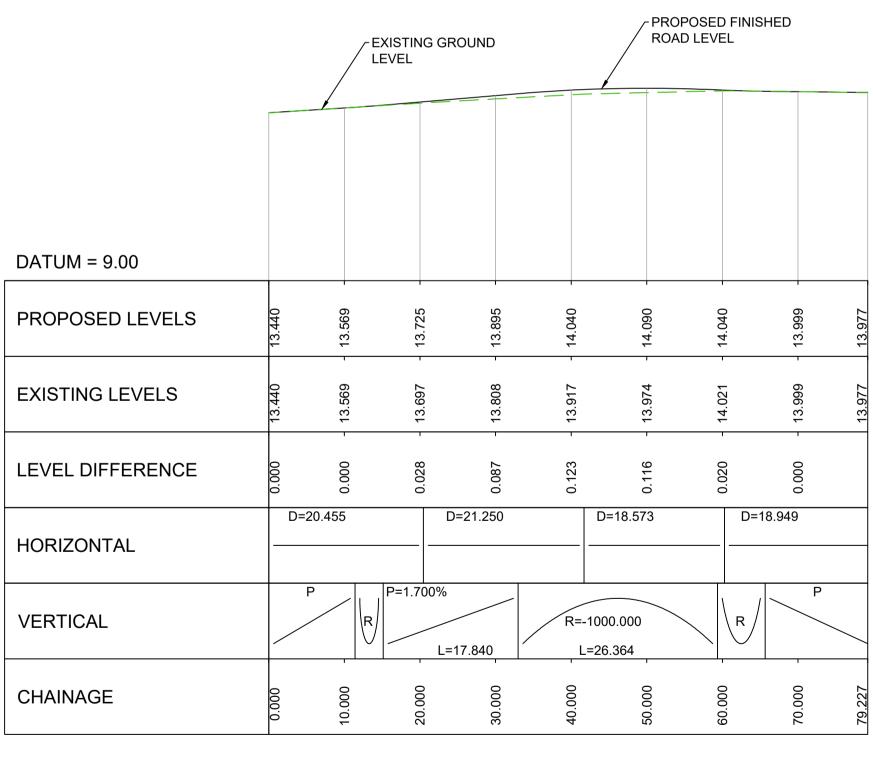
8.0



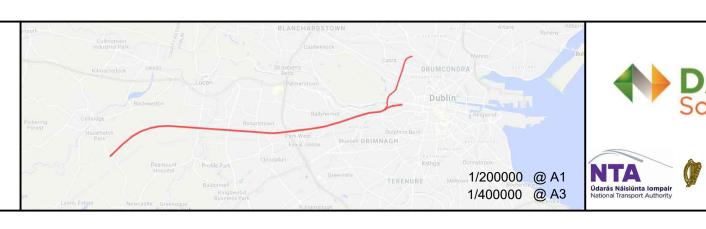


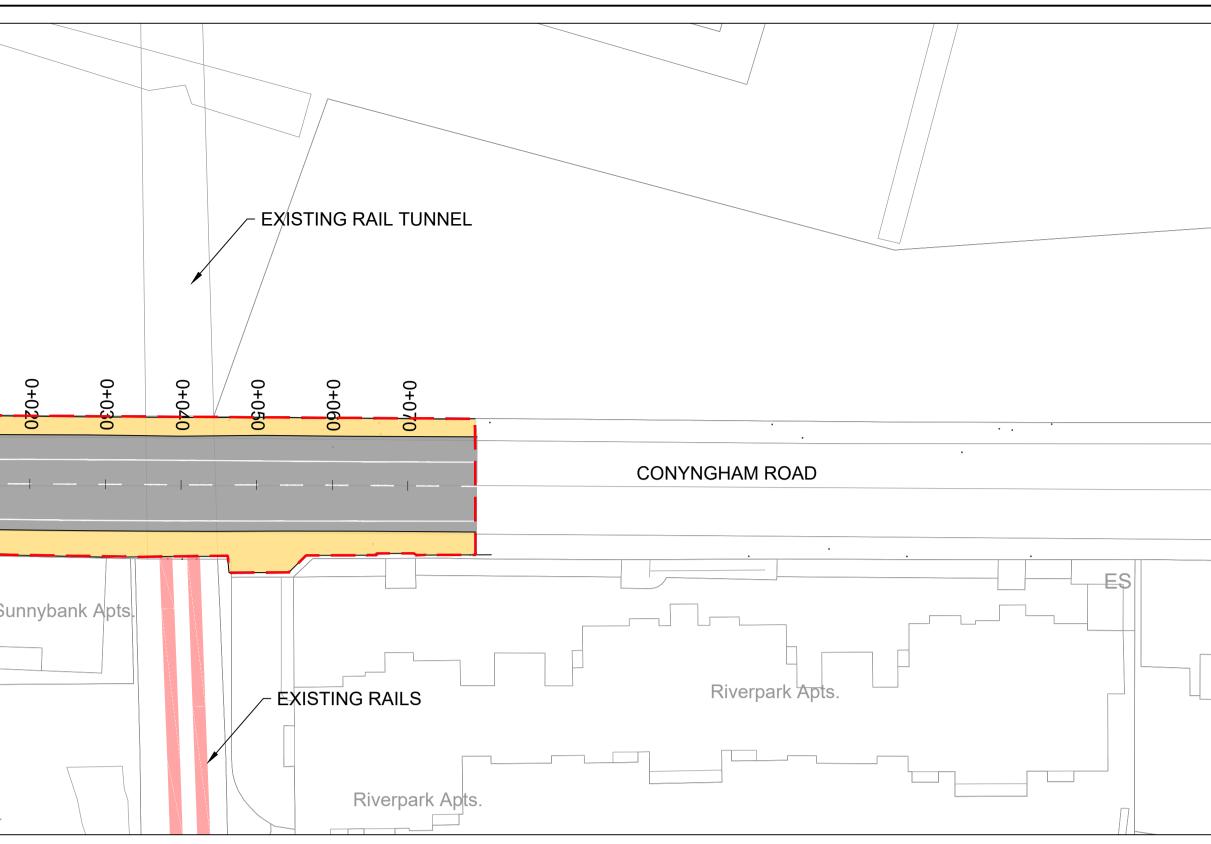
$ \begin{array}{ccccccccccccccccccccccccccccccccc$		Slab All di IRL2 Coef (Star - Trac - Gau - Cro - Ver Clear oper staff Vehit Toler	E: track is proposed in Option 1 istances in metres unless otherwise stated reference profile (Standard CME-TMS-327 A) ficients of the slab track tolerances adopted dard EN 15273-3. Gauges-Part 3: Structure ck position:0,005 mm ge widening:0,008 mm tical alignment variation:0,005 mm tical alignment variation:0,005 mm tical alignment variation:0,005 mm corpassengers to lean out. Le passing clearance between References P ance) to be 100mm as a minimum. It is assu- sseengers to lean out. IRL2 Reference Profile + Track	Vehicle Gauging) I for the kinemati Gauges): aintenance Toler m. It is assumed Profiles (+ Track M umed no window	ic gauge ance) and railway no windows for 1aintenance rs for staff
13.0 12.0 11.0 10.0 9.0 00.0	Project Title Drawing Title	PHOE	NOTE: INFORMATION IS PRESEN PUBLIC CONSULTATION NOT ALL DESIGN DETAIL OR CONS DETAIL ARE KNOWN AT THIS TIME LOCATIONS, CONSTRUCTION COM BRIDGE REQUIREMENTS, TEMPOF DIVERSIONS AND OTHER ELEMEN POTENTIAL INTERFERENCE WITH BASED ON THE LEVEL OF INFORM AVAILABLE AT THIS TIME. FURTHE DETAILED DESIGN AND TECHNICA RELATED SOLUTIONS WILL SEEK POTENTIAL INTERFERENCE WITH T + SOUTH WEST NIX PARK TUNNEL ECIONS (SHEET 2 of 2) OPTION 1	STRUCTION REI E, E.G. SUBSTAT MPOUNDS, TEM RARY UTILITY ITS PROPERTY RIG ATION AND DES ER WORK INCLU AL AND CONSTR TO MINIMISE PROPERTY RIG	TION PORARY SIGN JDING RUCTION SHTS
	Drawing File Name	DP-04-23-D	WG-PW-TTA-35760	Version V01	Status S3

		PHOENIX PARK		
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Bridge	Car Park		33	Su
Water Quay				



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PLAN - CONYNGHAM ROAD BRIDGE (OBO2 SCALE 1:500

	Rev	Date	Drn	Chk'd	App'd	Description	Client			Engineering D	Designer
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Tionscadal Éireann **** Project Ireland * *							Date 02.02.21	Scale	AS SH@ ()/ANI @ A3	Drawn DB	Checked JX
2040 ** ** Configured by the Constitute Existing	v01	21/04/2021	DB	JX	ТМ	FOR PUBLIC CONSULTATION No1	Project Code 5199586	lssuer	ATK	QMS Code	
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	LEGEND: PROPOSED BOUNDARY LIMIT - PERMANENT WORKS
	NOTE: INFORMATION IS PRESENTED FOR PUBLIC CONSULTATION NO. 1.
	 NOT ALL DESIGN DETAIL OR CONSTRUCTION RELATED DETAILS ARE KNOWN AT THIS TIME, E.G, SUBSTATION LOCATIONS, CONSTRUCTION COMPOUNDS, TEMPORARY BRIDGE REQUIREMENTS, TEMPORARY UTILITY DIVERSIONS AND OTHER ELEMENTS. POTENTIAL INTERFERENCE WITH PROPERTY RIGHTS IS BASED ON THE LEVEL OF INFORMATION AND DESIGN AVAILABLE AT THIS TIME. FURTHER WORK INCLUDING DETAILED DESIGN AND TECHNICAL AND CONSTRUCTION RELATED SOLUTIONS WILL SEEK TO MINIMISE POTENTIAL INTERFERENCE WITH PROPERTY RIGHTS.
CONYNGHAM	
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Project Title	DART+ SOUTH WEST
Drawing Title	NGHAM ROAD BRIDGE (OBO2) OPTION 2
Approved TM Drawing File Name DP-04	PLAN AND PROFILE -23-DWG-CV-TTA-35270 Version v01 Status S3
NLY	1 I J

LONGITUDINAL SECTION - CONYNGHAM ROAD BRIDGE (OBO2) SCALE 1:500 Horizontal, 1:100 Vertical