



Public Consultation No. 1: Consultation Findings Report

November 2021



Contents

| Chapter | Page |
|--|-----------|
| 1. Introduction | 1 |
| 1.1. DART+ Programme | 1 |
| 1.2. DART+ South West | 2 |
| 1.3. Public Consultation | 5 |
| 2. Public Consultation No. 1: The Emerging Preferred Option | 6 |
| 2.1. Overview of Public Consultation Process | 6 |
| 2.2. Ministerial Launch & Media Coverage | 6 |
| 2.3. Elected Member Briefings | 8 |
| 2.4. Stakeholder & Community Briefings | 8 |
| 2.5. Public Consultation No. 1 Leaflet | 9 |
| 2.6. Public Consultation No. 1 Brochure | 9 |
| 2.7. Letters to Potentially Affected Landowners | 9 |
| 2.8. Project Website | 9 |
| 2.9. Direct Correspondence via Emails, Online Forms and Project Helpline | 10 |
| 2.10. Meetings | 11 |
| 3. Response to Public Consultation No. 1 | 12 |
| 3.1. Assessment Methodology | 12 |
| 3.2. Overview of Submissions Received | 12 |
| 3.3. General Themes Raised During Consultation Process | 12 |
| 3.4. Specific Responses from the Feedback Form | 13 |
| 4. Summary of Feedback from Public Consultation Number 1 | 15 |
| 4.1. Project Need | 15 |
| 4.2. Environmental Impact Assessment | 17 |
| 4.3. Stations | 24 |
| 4.4. Project Benefits | 31 |
| 4.5. Consultation & Engagement | 33 |
| 4.6. Design | 34 |
| 4.7. Policy and Planning | 35 |
| 4.8. Construction | 39 |
| 4.9. Electrification (OHLE, Substations etc.) | 43 |
| 4.10. Landownership | 43 |
| 4.11. Bridges | 46 |
| 4.12. Safety | 48 |
| 4.13. Operational Phase / Post Construction | 49 |
| 4.14. Four tracking | 51 |
| 4.15. Surveys & Site Investigations | 52 |
| 5. Next Steps | 53 |
| 5.1. Summary | 53 |
| 5.2. Next Steps | 56 |
| Appendices | 57 |
| Appendix A. Sample Media Coverage | 58 |
| Appendix B. Sample Presentation to Elected Members & Other Stakeholders | 67 |

| | | |
|--------------------|---|------------|
| Appendix C. | PC1 Leaflet and Brochure | 79 |
| Appendix D. | Website Screenshots and Frequently Asked Questions | 113 |
| Appendix E. | Virtual Consultation Room Panels | 120 |

Tables

| | |
|--|----|
| Table 2-1 - Public Consultation No. 1 webinar meetings | 8 |
| Table 3-1 - Level of Engagement with PC1 | 12 |
| Table 3-2 - Themes of Feedback and Number of References Received | 13 |

Figures

| | |
|---|----|
| Figure 1-1 - Schematic diagram of DART+ Programme extent | 2 |
| Figure 1-2 - DART+ South West Capacity Increase | 3 |
| Figure 1-3 - Map of proposed DART+ South West project | 4 |
| Figure 2-1 - Photograph from the Ministerial Launch | 7 |
| Figure 2-2 - Twitter and Facebook posts by Iarnród Éireann advertising the public consultation | 8 |
| Figure 2-3 - Virtual consultation room developed as part of online public consultation experience | 10 |
| Figure 3-1 - Response to Question 2A | 14 |
| Figure 3-2 - Response to Question 6A | 14 |

1. Introduction

1.1. DART+ Programme

DART+ Programme is a key transportation improvement to form a high quality and integrated public transport system. It will have benefits for the residents of the Greater Dublin Area and also those living in the other regions. It will assist in providing a sustainable transport system and a societal benefit for current and future generations.

The current electrified DART network is circa 50km long, extending from Malahide / Howth to Bray / Greystones, and the DART+ Programme seeks to increase the high capacity and electrified network to network to 150km. The DART+ Programme is required to facilitate increased train capacity to meet current and future demands, which will be achieved through a modernisation of the existing railway corridors. This modernisation includes the electrification, re-signalling and certain interventions to remove constraints across the four main rail corridors within the Greater Dublin Area, as per below:

- DART+ South West (this Project) – circa 16km between Hazelhatch & Celbridge Station to Heuston Station and also circa 4km between Heuston Station to Glasnevin, via the Phoenix Park Tunnel Branch Line.
- DART+ West – circa 40km from Maynooth & M3 Parkway Stations to the City Centre.
- DART+ Coastal North – circa 50km from Drogheda to the City Centre.
- DART+ Coastal South – circa 30km from Greystones to the City Centre.
- DART+ Fleet – purchase of new electrified fleet to serve new and existing routes.

The DART+ Programme is a key element to the national public transportation network, as it will provide a high-capacity transit system for the Greater Dublin Area and better connectivity to outer regional cities and towns. This will benefit all public transport users.

Delivery of the DART+ Programme will promote transport migration away from the private car and to public transport. This transition will be achieved through a more frequent and accessible electrified service, which will result in reduced road congestion, especially during peak commuter periods.

The DART+ Programme will provide enhanced, greener public transport to communities along the DART+ Programme routes delivering economic and societal benefits for current and future generations.

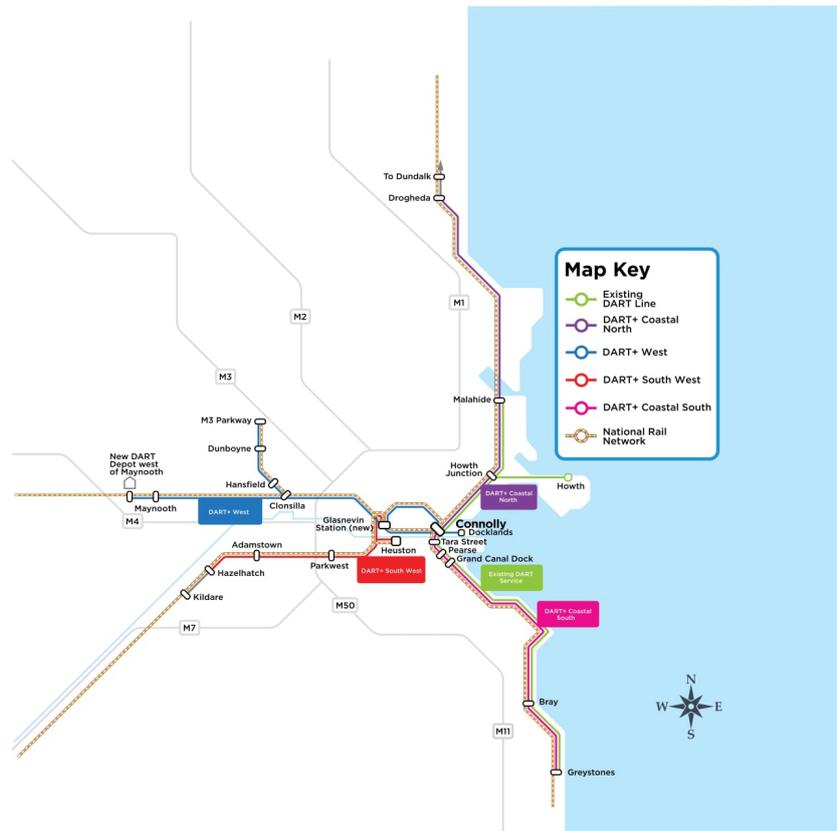


Figure 1-1 - Schematic diagram of DART+ Programme extent

1.2. DART+ South West

The DART+ South West Project will deliver an electrified network, with increased passenger capacity and enhanced train service between Hazelhatch & Celbridge Station to Heuston Station (circa 16km) on the Cork Mainline, and to Glasnevin on the Phoenix Park Tunnel Branch Line (circa 4km).

DART+ South West Project will complete four tracking between Park West & Cherry Orchard Station and Heuston Station and will also re-signal and electrify the route. The completion of the four tracking will remove a significant existing constraint on the line, which is currently limiting the number of train services that can operate on this route. DART+ South West will also deliver track improvements along the Phoenix Park Tunnel Branch Line, which will allow a greater number of trains to access the city centre.

Upon completion of the electrification of the DART+ South West route, new DART trains will be used on this railway corridor, similar to those currently operating on the Malahide / Howth to Bray / Greystones Line.

1.2.1. Capacity Increases Associated with DART+ South West

The operating capacity of services in the Heuston area is currently constrained by railway infrastructure limitations and the ability of Heuston Station to accommodate terminating trains. Iarnród Éireann currently operates at a maximum capacity of 12 inbound trains in the AM peak hour and 12 outbound trains in the PM peak hour along the Cork Mainline. This provides a peak capacity of approximately 5,000 passengers per hour per direction during the AM and PM peak hours; operating inbound and outbound, respectively. DART+ South West aims to improve train service and increase train and passenger capacity on the route between Hazelhatch & Celbridge Station to Heuston Station and through the Phoenix Park Tunnel Branch Line to the City Centre, covering a distance of circa 20km.

DART+ South West will significantly increase train capacity from the current 12 trains per hour per direction to 23 trains per hour per direction (i.e., maintain the existing 12 services, with an additional 11 train services provided by DART+ South West). This will increase passenger capacity from the current peak capacity of approximately 5,000 passengers per hour per direction to approximately 20,000 passengers per hour per direction.



Figure 1-2 - DART+ South West Capacity Increase

1.2.2. Key infrastructural elements of DART+ South West

The key elements of DART+ South West include:

- Completion of four-tracking from Park West & Cherry Orchard Station to Heuston Station, extending the works completed on the route in 2009.
- Electrification of the line from Hazelhatch & Celbridge Station to Heuston Station and also from Heuston Station to Glasnevin, via the Phoenix Park Tunnel Branch Line, where it will link with proposed DART+ West.
- Undertaking improvements/reconstructions of bridges to achieve vertical and horizontal clearances.
- Remove rail constraints along the Phoenix Park Tunnel Branch Line.
- Delivery of a new Heuston West Station¹.

The Preferred Option will be compatible with future stations at Kylemore and Cabra, although the construction of these stations is not part of the DART+ South West Project.

Figure 1-3 below shows a map of the extent of the DART+ South West project.

¹ For PC1 the scope of the project involved feasibility of a new Heuston West Station. As a result of stakeholder feedback, the new station will now be brought forward to Railway Order. Refer to **Section 4**.

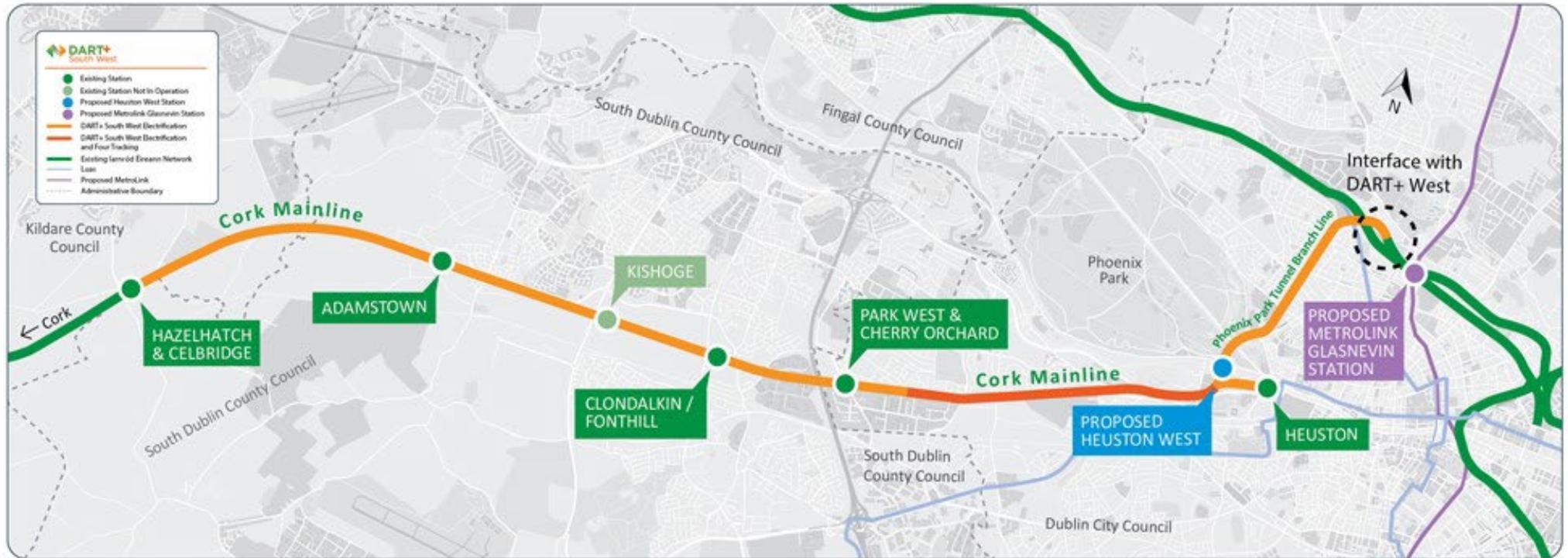


Figure 1-3 - Map of proposed DART+ South West project

1.3. Public Consultation

Public participation is a key element to the delivery of major infrastructure projects, such as the DART+ South West project. The purpose of public consultation is to engage the public in the scheme delivery process; inform the public of the statutory process and the likely timescales; seek the public's cooperation and understanding of the project; and to capture local knowledge to inform the Environmental Impact Assessment (EIA) and Railway Order (RO) process.

Public participation is welcomed and encouraged throughout the design development process. However, there will be three main project consultation stages which will provide the opportunity to learn about the design development and provide feedback which will inform the next stage as appropriate. The main public participation stages as part of the project development are illustrated below:

- Non-Statutory Public Consultation No.1 The Emerging Preferred Option (Spring 2021)
- Non-Statutory Public Consultation No.2 Preferred Option (Winter 2021)
- Statutory Consultation Period as part of the Railway Order application process (Summer/Autumn 2022)

This report details the process and records and analyses the feedback from the first of the public consultation events, Public Consultation No. 1 The Emerging Preferred Option.

1.3.1. COVID-19

Due to ongoing COVID-19 restrictions, which limited the number of people that could attend events, Public Consultation No.1 focused predominantly on digital / online consultation. In order to comply with the public health restrictions consultation, including meetings with affected landowners / residents were predominantly conducted online (website / email / Microsoft Teams / telephone). The project team held seven online public webinars for residents local to the affected areas, including: Heuston to Kilmainham, Inchicore to Kylemore, Ballyfermot, Clondalkin to Adamstown, Celbridge & Hazelhatch, Cabra and further meetings for all surrounding communities (see **Table 2-1**).

2. Public Consultation No. 1: The Emerging Preferred Option

2.1. Overview of Public Consultation Process

This public consultation findings report has been prepared to summarise and assess the feedback received from Public Consultation No. 1 on the Emerging Preferred Option. The consultation period commenced on 12th May 2021 and ran for 6 weeks until its formal closure on 23rd June 2021. However, it should be noted that an additional week to 30th June 2021 was provided to allow stakeholders to engage and submit feedback on the Emerging Preferred Option as part of Public Consultation No. 1.

As described in the public consultation brochure, Public Consultation No. 1 was an opportunity for potential users of the services, those likely to be affected by its development and all members of the general public, to express their views on the project plans at an early stage in the design process. On projects such as DART+ South West, local knowledge communicated through submissions of all types, positive or negative, informs the design development process.

The feedback and engagement, summarised in this report, will ultimately assist the project team in improving the project and will ensure the successful delivery of a project that best meets the needs of its users and the local communities. Throughout Public Consultation No. 1, the project team responded to all queries raised in a timely manner. The objective was to assist the public in gaining a better understanding of the project and to encourage engagement in the consultation process. The following sections describe the various channels of communication used to notify and inform the public of Public Consultation No. 1.

2.2. Ministerial Launch & Media Coverage

Public Consultation No. 1 was launched by the Minister for Transport, Eamon Ryan TD on 12th May 2021 (**Figure 2.1**). Iarnród Éireann Corporate Communications and Media team provided a press release to all major media outlets and the launch was covered widely on the day by national media including:

- Irish Independent
- The Irish Times
- Irish Daily Mail
- Irish Daily Mirror
- Irish Sun
- RTÉ Radio 1
- Newstalk
- Breakingnews.ie
- Journal.ie

A selection of press clippings in relation to the launch are provided in **Appendix A**.



Figure 2-1 - Photograph from the Ministerial Launch

2.2.1. Advertising

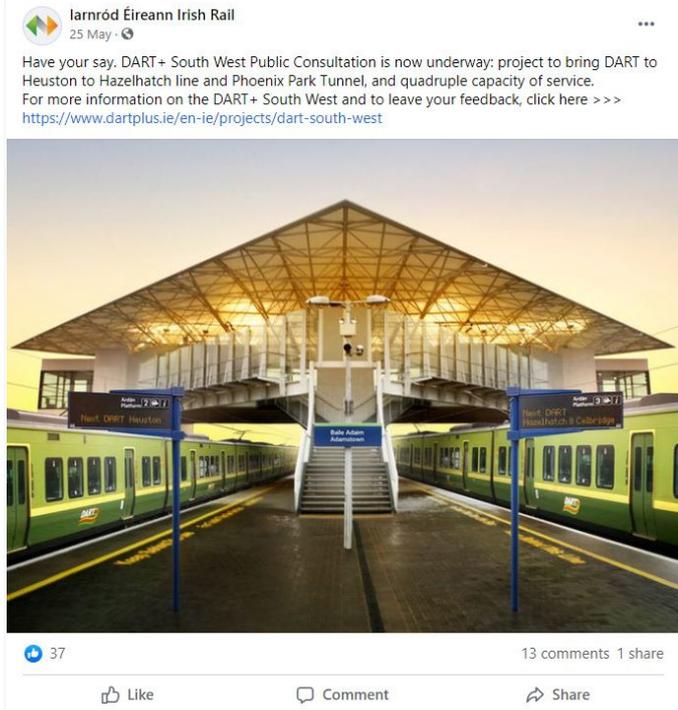


Figure 2-2 - Twitter and Facebook posts by Iarnród Éireann advertising the public consultation

On the afternoon of 12th May 2021, immediately following the ministerial launch event, a series of online briefing sessions were held for 51 elected representatives from Dublin City Council, South Dublin County Council and Kildare County Council.

The presentation provided during the online briefing sessions can be found in **Appendix B**.

2.4. Stakeholder & Community Briefings

Seven online public webinars were held during Public Consultation No. 1. The purpose of the webinars was to answer any questions the public had on the project to assist them in writing a formal submission. During each webinar a presentation on the Emerging Preferred Option and the public consultation process was given, followed by a question-and-answer section, for attendees to raise their questions with the project team. The presentations followed a general format but were tailored for specific geographic locations. Participants of the webinars were encouraged to make a formal submission via email, post or website channels, as part of the consultation process. **Table 2.1** provides a list of webinars and presentations held.

Table 2-1 - Public Consultation No. 1 webinar meetings

| Date | Communities |
|---|------------------------------|
| Tuesday, 18 th May at 19:00hrs | Heuston to Kilmainham area |
| Wednesday, 19 th May at 19:00hrs | Inchicore to Kylemore area |
| Thursday, 20 th May at 19:00hrs | Ballyfermot area |
| Tuesday, 25 th May at 19:00hrs | Clondalkin to Adamstown area |

| | |
|---|--|
| Wednesday, 26th May at 19:00hrs | Celbridge and Hazelhatch area |
| Thursday 27th May at 19:00hrs | Cabra area |
| Thursday 17th June at 19:00hrs | Additional meeting for all surrounding communities |

2.5. Public Consultation No. 1 Leaflet

An information leaflet, printed in both English and Irish, highlighting the key elements of the project was circulated to residents in the project areas. Over 24,000 properties along the project corridor received a leaflet which notified the local communities of the consultation period and how to contact the project team. The leaflet was made available in both Irish and English languages on the project website.

The English leaflet can be found in **Appendix C**.

2.6. Public Consultation No. 1 Brochure

A 54-page non-technical public consultation brochure, presenting the key details of the DART+ South West project, the benefits, the option selection process and the Emerging Preferred Option was developed and published online in both the Irish and English languages. The brochure was made available on the dedicated project webpage and hard copies were issued to the elected representatives following the ministerial launch.

The English brochure can be found in **Appendix C**.

2.7. Letters to Potentially Affected Landowners

Letters to identified properties likely to be affected by the permanent footprint of the Emerging Preferred Option (17 in total) were sent via registered post notifying them in advance of the commencement of Public Consultation No. 1.

Contained in the letter was a brief overview of the project, a notification that the property had been identified to be likely affected by the Emerging Preferred Option and an invitation for the recipient to contact the project team to arrange a meeting to provide further information. Engagement with the potentially affected landowners is ongoing..

2.8. Project Website

A dedicated project webpage was established on the DART+ Programme website (www.DARTplus.ie) which presented all of the project information published as part of Public Consultation No. 1 including the project leaflet (English and Irish), brochures (English and Irish), the feedback form, the Preliminary Options Selection Report and associated annexes and drawings. A Frequently Asked Questions (FAQ) sheet was developed following initial submissions received and was published on the project website. The FAQ document was regularly updated throughout the consultation period to reflect additional issues that were raised during the process. A screenshots of the project website along with the FAQ sheet published for the consultation can be found in **Appendix D**.

2.8.1. Virtual Consultation Room

Due to the COVID-19 restrictions in place at the time of Public Consultation No. 1, an in-person public consultation event was unable to be held. Instead, a virtual consultation room containing all the information that would normally be displayed at a live event was developed and made available on the project website as part of the online public

consultation experience. The virtual consultation room allowed the public and other stakeholders to view maps, project information and other relevant information in a safe and accessible environment. **Figure 2.3** shows an image of the virtual consultation room which can be accessed via: <https://www.DARTplusvr.ie/>. The panels are included in **Appendix E**.

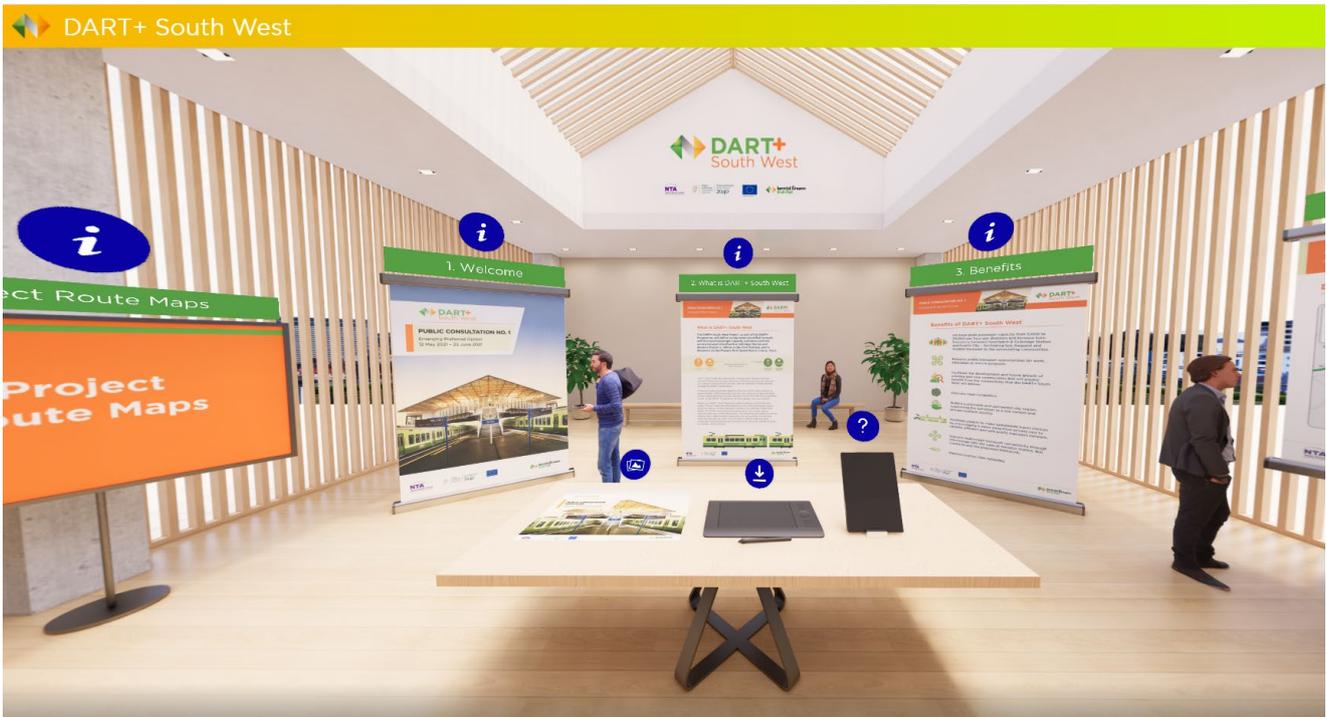


Figure 2-3 - Virtual consultation room developed as part of online public consultation experience

2.8.2. Project Webpage Analytics

The webpage analytics measures the hit rate on the public consultation webpage as well as the areas of the webpage where the most traffic was received. The use of these analytics was in compliance with GDPR.

The analytics demonstrated that the project webpage had a total of **45,568** separate page views between the 12th May 2021 and the 30th June 2021.

The top downloads from the project website were the project leaflet, the project brochure, preliminary option selection – main report, preliminary options selection report – executive summary and the Emerging Preferred Option key plan map.

2.9. Direct Correspondence via Emails, Online Forms and Project Helpline

A project email address (DARTSouthWest@irishrail.ie) and a project postal address were provided on all project materials. An online feedback form was provided on the project webpage to allow the public to make submissions on the project. The online feedback form asked respondents specific questions relating to the project, asking them to provide comments, suggestions, ideas and to detail what aspects of the project were of interest to them.

A helpline was established to ensure that all calls received during the consultation period were answered, documented, passed to the dedicated Community Liaison Officer (CLO), and promptly responded to. A postal

address was also provided for stakeholders to make submissions in hard copy. A breakdown of the correspondence received is included in **Table 3-1** below.

All the above measures were promoted to ensure adherence with the public health advice in relation to COVID-19.

2.10. Meetings

Meetings were arranged with affected landowners and attended by the CLO, Design team and CIÉ Group Property Representatives. As well as the phone calls made to the affected landowners, a total of 4 landowner meetings took place. Meetings were held virtually on Microsoft Teams due to safety precautions as a result of COVID-19.

3. Response to Public Consultation No. 1

3.1. Assessment Methodology

All submissions received either via email, post, telephone, or through the online feedback form were analysed and recorded by the project team on a dedicated consultation database. Each individual submission was analysed to identify the themes that were raised by the respondent and each submission was classified according to the themes raised. All feedback provided was then anonymised before being analysed under each of the themes. A detailed summary of the feedback provided by stakeholders is presented below in **Section 4** of this report.

The online feedback forms posed specific questions in relation to the proposed project namely, questions 2A and 6A. The responses to these questions are assessed in **Section 3.4** below.

3.2. Overview of Submissions Received

During PC1, the project team received 1,003 unique submissions from stakeholders. In addition, a further 2 petitions supported by 254 stakeholders set out specific local considerations and concerns in respect of the need for a station at Cabra.

Submissions were received across all the channels made available for the consultation. A breakdown of the engagement by channel is provided in **Table 3-1** below. Submissions were accepted until 30th June 2021 providing stakeholders with an additional week to provide submissions beyond the original consultation closing date.

Table 3-1 - Level of Engagement with PC1

| Channel | Cumulative |
|----------------------------------|-----------------------------|
| Phone Calls | 41 |
| Emails | 252 |
| Feedback Forms | 708 |
| Post | 2 |
| Petitions | 2 (126 and 128 signatories) |
| Virtual Consultation Room Visits | 13,592 |
| Website Views | 45,568 |

In addition to the above engagement, further engagement with relevant Local Authorities and prescribed bodies has been ongoing. Engagement with potentially affected landowners has also taken place since the commencement of PC1.

3.3. General Themes Raised During Consultation Process

Feedback received during the consultation has been collated into 15 themes in order to present the information in an accessible manner. **Table 3-2** below provides an overview of the themes and the number of references made in the feedback to each theme. The feedback given under each theme is summarised in **Section 4** of this report.

Table 3-2 - Themes of Feedback and Number of References Received

| Feedback Theme | Number of References in Feedback |
|---------------------------------------|----------------------------------|
| Project Need | 1,221 |
| Environmental Impact Assessment | 1,134 |
| Stations | 1,121 |
| Project Benefits | 976 |
| Consultation & Engagement | 304 |
| Design | 294 |
| Policy & Planning | 253 |
| Construction | 212 |
| Electrification | 114 |
| Landownership | 102 |
| Bridges | 96 |
| Safety | 85 |
| Operational Phase / Post Construction | 82 |
| Four Tracking | 47 |
| Surveys & Site Investigations | 12 |

3.4. Specific Responses from the Feedback Form

As part of PC1 for the project, a feedback form was provided on the project website to encourage participation in the public consultation. The form sought feedback on a variety of topics and allowed stakeholders to provide their views via free text boxes. Feedback provided via these free text boxes, was assessed by the project team and is included in the feedback summary that is provided in **Section 4** below.

Two specific queries were asked in the feedback form and the responses received are set out below:

Question 2A: Do you Support the principle of the DART+ South West project?

Figure 3-1 below, shows that of the 708 respondents, 646 or 91% were in support of the principle of the project. 56 respondents, or 8% indicated that they did not support the scheme, while 6 respondents, or 1%, chose not to answer.

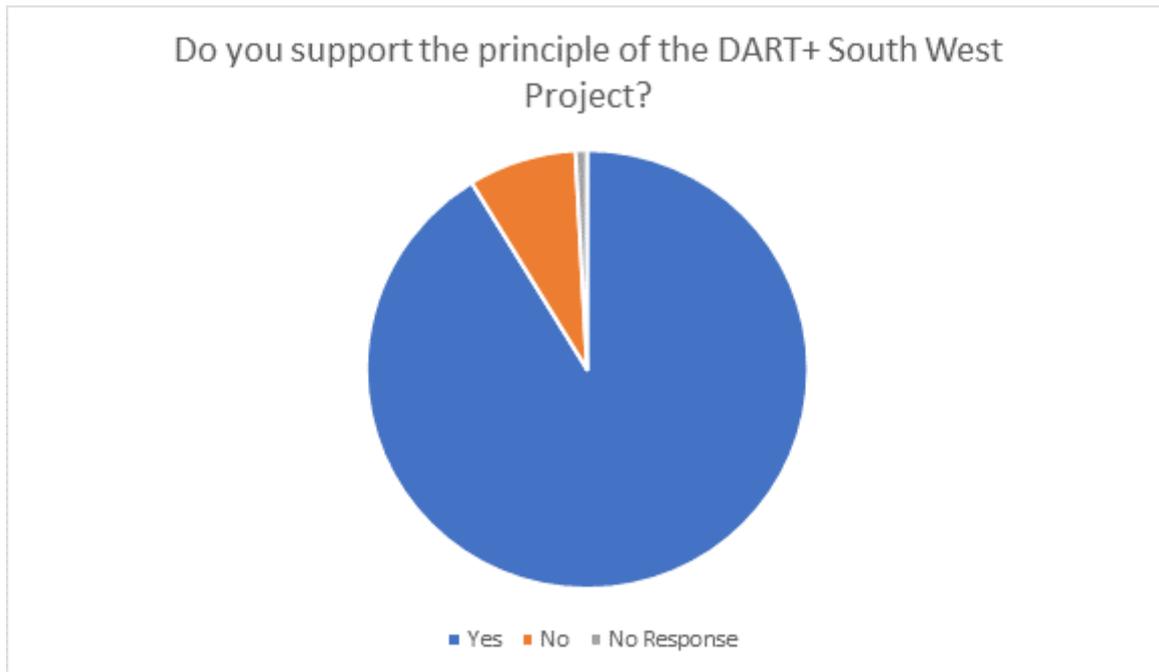


Figure 3-1 - Response to Question 2A

Question 6A: Will the improved services encourage you to change from travelling by private car to public transport?

Figure 3-2 below, shows that of the 708 respondents, 528 or 75% said that improved services would encourage them to change from travelling by private car to public transport. A further 158 respondents, or 22% indicated that the scheme would not encourage such a change. Finally, 22 respondents, or 3%, chose not to answer.

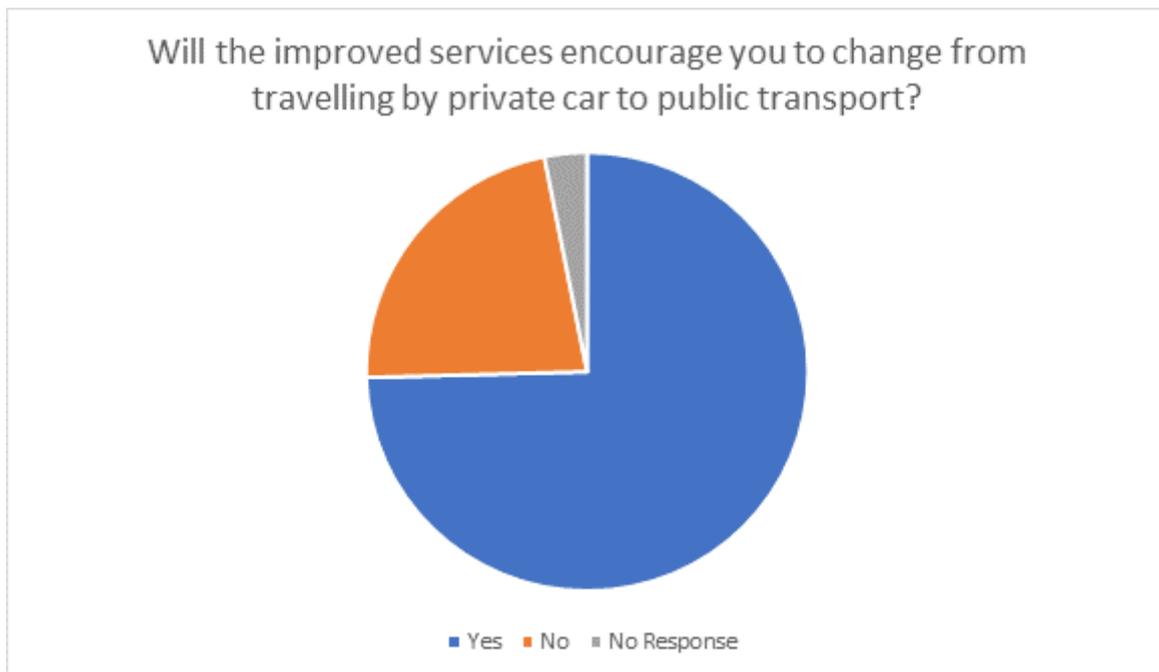


Figure 3-2 - Response to Question 6A

4. Summary of Feedback from Public Consultation Number 1

Feedback received during the consultation has been collated into the following 15 themes and is summarised in this section of the report:

- Project Need
- Environmental Impact Assessment
- Stations
- Project Benefits
- Consultation & Engagement
- Design
- Policy & Planning
- Construction
- Electrification
- Landownership
- Bridges
- Safety
- Operational Phase / Post Construction
- Four Tracking
- Surveys & Site Investigations

The feedback presented in Section 4 reflects the comments received by the project team and does not represent Iarnród Éireann's views on the particular issues. It is presented to show the broad nature of feedback provided and to ensure that the project has regard to the views presented during the consultation.

4.1. Project Need

Project Need received the highest number of references within the submissions submitted in PC1. Submitters recognised the “need” for the project, citing several reasons which indicated that this project is a “necessity” within Ireland's upcoming infrastructure projects. The information received in submissions regarding Project Need have been further sub-categorised into the following sections: Project Need and Timeline; Journey Time, Frequency and Capacity of Trains; Connectivity and Accessibility; Population and Development; Climate Action; Public Health; and Project Design and Stations.

4.1.1. Project Need and Timeline

A number of submissions referred to the timeline of the project or expressed general support for the “need” for this project to go ahead.

Some stakeholders requested the acceleration of the project timeline to ensure that the DART is a “first-class transport system for a smart growing city”. Stakeholders suggested the project team need to just “build it”, expressing there is an urgent need for the service.

Stakeholders suggested that the development of housing developments and new high-rise buildings will increase the amount of people using the proposed rail line. They highlighted the “need” for the works to be completed to prevent these areas becoming reliant on single occupancy vehicles.

4.1.2. Journey Time, Frequency and Capacity of Trains

Feedback stated that the project was needed to reduce the journey time between different parts of the city. Respondents welcomed the increase in DART capacity, through the DART+ SW project, stating that current services are too overcrowded.

It was suggested the enhanced capacity would “encourage modal change, reduce reliance on private cars and transform Dublin into a resilient city”. Stakeholders also indicated with more capacity, there would be increased reliability of the DART trains which would lead to faster commuting times, saving time for its users.

4.1.3. Connectivity and Accessibility

A number of submissions which referenced Project Need, referred to Connectivity and Accessibility as an area of interest.

Stakeholders suggested the project is necessary to promote interconnectivity between transport options in the city such as the LUAS, BusConnects, MetroLink and the DART+ Programme. In some feedback, Dublin was described as “lacking a comprehensive and efficient mass public transport network”. Submitters described the need to make different parts of the city more accessible and for a public transport system that connects “all parts of the city” and that is attractive for commuters.

It was suggested the DART+ SW project is needed to encourage connectivity of multi-modal transport in the city, including for cyclists and pedestrians. Respondents expressed hope that the project would support “the seamless integration with cyclists and bus services”.

Stakeholders outlined that the electrification of the Phoenix Park Tunnel, as well as the project’s integration with DART+ West and Metrolink, are positive attributes of the project, as they will allow for full flexibility of the transport options within the city.

Respondents expressed support for the project due to the investment of the DART in the Celbridge area. It was suggested the project will “open up the line to tens of thousands more people” and offer them a “compelling and reliable public transport option”.

Stakeholders also suggested that the project will benefit people with disabilities and those from more socially disadvantaged backgrounds with their commute, as the DART+ SW line will improve access to additional work and travel locations.

4.1.4. Population and Development

Feedback cited that the project is needed for the benefit of the future populations of the local areas which the line will serve. Stakeholders noted development of the area, both presently and in the future, will make a considerable impact to the surrounding areas, its residents and commuters.

Respondents highlighted that the project is needed to provide increased space in the city centre. It was suggested with reduced numbers of cars, cyclists and pedestrians would have more space, as more people will be using public transport.

4.1.5. Climate Action

A number of submissions which referenced Project Need, mentioned Climate Action as a reason for “needing” this project.

Stakeholders expressed the belief that there is a need for climate action in Ireland, which this project will contribute to with “clean” transport. Respondents suggested that many car users would switch mode of transport to use the DART line, therefore helping to reduce emissions.

Electrification of the line was welcomed in feedback given the reduction in noise pollution. Stakeholders noted Dublin’s need to reduce pollution levels and carbon emissions, which this project will help address. The overall climate benefits were noted by stakeholders as an important aspect of the project.

Feedback outlined that “the Government’s Climate Action Plan lists the DART expansion [DART+ Programme] as a major sustainable mobility project which will have a significant impact in empowering a modal shift”.

Some stakeholders noted that it is the ‘European Year of the Train’ in 2021 and “in that spirit we should show our ambition in climate action supporting sustainable travel and in order to empower people to choose public transport rather than their car, we need to design a system that is easily accessible, fast and reliable”.

4.1.6. Public Health

Submissions were received which commented on the projects impact regarding public health. Respondents described the public health benefits of the project including bettering the overall health and wellbeing for “everyone in the public realm” due to reduced pollution and improved air quality.

Stakeholders noted the area surrounding the works are home to many young families and noted the importance of ensuring the area remains clear of air pollution exacerbated by the proposed works.

4.1.7. Project Design and Stations

The need for more stations was referenced in submission received and outlined specific requests / suggestions for the project design.

Respondents suggested that the DART Underground needs to be included in the full DART+ Programme as it “ties everything together”. Some stakeholders suggested the project “lacked ambition and foresight” and was the “shortest DART line of all, which starts and terminates within County Dublin and excludes key stations”.

With regard to stations, many submissions stated that the project “does not go far enough” and respondents expressed support for further stations along the line to “encourage development” and serve areas with large populations. It was suggested that more stations would be needed to encourage more people to use the line, rather than relying on their cars.

4.2. Environmental Impact Assessment

4.2.1. Air Quality

Respondents expressed concern regarding an increase in brake dust due to the increase in trains on the line. Some stakeholders complained about the current levels of brake dust causing them to keep their windows closed.

Concern was noted that with an increase in trains on the line, this issue will worsen with an increase in dust particles in the air. Stakeholders noted that they are unwilling to accept any worsening in air pollution as a result of these proposed works.

It was outlined an area near the proposed works, was found to have exceeded EU standards for air pollution levels, in a study conducted by the EPA. Respondents outlined that in a further study conducted by UCD, the areas around the works were recorded as “red zones for air pollution”. One of the areas studied, Sarsfield Road, recorded an “acutely high volume of traffic passing through a high-density residential area”. Stakeholders expressed concern that the works and increase in train activity will “only worsen the situation”. Respondents outlined that previous track works saw the removal of trees in the area. Given the importance of these trees for air quality, residents do not want this repeated.

To ensure that the works and subsequent increase in train activity, do not increase air pollution, stakeholders suggested that air quality surveys be conducted near residential sites, before, during and after completion of the works, to ensure air pollution levels do not rise or cause harm to residents. Some stakeholders suggested that in order to improve service, the diesel fleet of trains will run more frequently, thus having an increased effect on pollution levels in the area.

However, some stakeholders stated that the shift in transportation usage from private cars to public transport, therefore reducing the number of single-occupancy vehicles, as well as the electrification of the line, “will help reduce air pollution and ease congestion”.

4.2.2. Alternatives

Stakeholders noted that the proposed works should be planned “appropriately” to integrate other transport options, by enhancing facilities available along the proposed line and at stations. Submissions noted that this improved service could be facilitated by diesel or by electric trains, therefore regarding electrification of the line as potentially unnecessary.

4.2.3. Archaeology and Cultural Heritage

Stakeholders noted that the proposed works and subsequent line, will pass through areas of historical and cultural significance. Stakeholders expressed concern regarding the possible impact of the project on heritage sites. It was suggested the project engage with appropriately registered and trained archaeologists to ensure an accurate Heritage Impact Assessment Report (HIAR) is conducted. Stakeholders outlined the HIAR will ensure an “inventory of the historical architectural / cultural sites and structures along the proposed route are not impacted”. Stakeholders suggested the “city archaeologist” could inform the design team of any possible historical / cultural impacts, and that engagement with them is key. Submissions requested that the HIAR encompasses the impact of the works across all sites involved, both temporary and permanent.

The area surrounding Glasnevin was highlighted by respondents for its cultural importance. Stakeholders want to ensure assessments are undertaken to ensure no structure or feature is knowingly or unknowingly damaged as a result of the proposed works. Some stakeholders suggested an Underground DART line would impact less on historical sites in the area.

4.2.4. Architectural Heritage

Respondents expressed concern that within the area of the proposed works, there are various buildings and bridges of significant architectural heritage importance.

In order to ensure that no unnecessary damage to structures occurs, stakeholders suggested an architectural assessment be undertaken by a trained and registered Conservation Architect. Many submitters believe that the architect should be engaged with before, during and after the works to ensure appropriate conservation takes

places. Submissions noted that this assessment should inform the overall Heritage Impact Assessment Report (HIAR).

Stakeholders highlighted the Iarnród Éireann Inchicore Works Estate as a particularly important area that should be assessed, given its various protected structures on the grounds. Stakeholders noted that the current plans encompass an 1860's wall, foot / road bridges, and the 19th Century turret and signal box, that should be conserved. Respondents stated that a compulsory purchase order (CPO) had been given to residents in the area and expressed aggrievance that a listed wall was due for demolition. Concerned respondents requested that if buildings of architectural heritage must be impacted for the works to take place, that they be carefully "disassembled, stored and then reassembled later in a suitable location".

Submissions noted that anything deemed architectural heritage should be included within the scope of social heritage. To illustrate this, the example of the Old Cabra Road Bridge was given, which was a site of engagement during the 1916 rising, encompassing both architectural and cultural heritage.

4.2.5. Biodiversity

Safeguarding local biodiversity and the conservation of green space were important issues expressed by stakeholders. It was outlined that procedures should be in place for the project design and implementation to protect the local environment.

Respondents stated the need for an ecological survey to take place before, during and after construction to ensure minimal impact on the flora and fauna. It was suggested that the areas surrounding the current tracks are natural hotspots for biodiversity, as rail lines have uncultivated ground which can act as a refuge for the fauna through built up areas.

Stakeholders noted the presence of a community orchard as a "key area of preservation", which they hope will remain untouched by the proposed works. It was suggested that if essential works must interfere with the garden that it should be "fully restored after". It was also suggested by stakeholders that the proposed works will affect a Dublin City Council supported walled residents garden, which was zoned Z2 under the 2016-2022 Development plan.

Respondents outlined that previous track maintenance works saw the removal of trees in the area. It was noted that if trees must be removed, stakeholders wish for their replacement as well as more trees added in margins in between used areas.

Some stakeholders noted concern due to a possible rise in rodent activity during and after the works, due to disruption to their habitat.

Stakeholders expressed concerns about the proposed line and its possible impact on the Phoenix Park. Respondents stated that they do not want the line to infringe on the park, "environmentally or physically".

4.2.6. Climate

The importance of Ireland reaching its climate goals was stressed by respondents. Many believe an electric train line will put Ireland in a better position to achieve the EU goals set.

Stakeholders noted the need for "greener" transportation options in Dublin. It was suggested the move from diesel to electric trains would lessen air pollution, allowing for a healthier environment.

Respondents suggested that "Greener", "quicker", and "quieter" electric transport, would encourage commuters to use public transport driving on over-congested roads. Stakeholders suggested the proposed works would have a "positive impact on the environment" as they would encourage road users to use the train. Submitters noted that "any efforts to reduce the number of cars on the road" is positive.

Some stakeholders noted that as most of the fleet are still diesel powered, and the electric trains may be powered by coal burning power stations, it is unlikely air pollution would lessen. Respondents suggested that for the system to be efficient, “investments should be made in renewable energy power sources”. Stakeholders requested that any project work must take air, noise and long-term pollution impacts into consideration.

Respondents also outlined concern that the consequences of climate change, increased rainfall; increased flooding; and rising sea levels, could impact both this service and future rail developments.

4.2.7. EIA Process / Methodology

Stakeholders wanted assurances that an appropriate Environmental Impact Assessment (EIA) would be carried out. They noted that it should be “robust” in nature, particularly regarding noise levels in the area. Submitters also requested that detailed floral and faunal studies are conducted, to assess the possible ecological impacts of the proposed works. It was suggested that monitoring studies in designated environmentally sensitive areas, would ensure the impacts of the works would not be harmful to the environment long-term. Respondents stated that the cumulative impacts of construction traffic and proposed traffic management measures will have to be addressed in the EIAR for the Railway Order application.

4.2.8. Electromagnetic Compatibility and Stray Current

Stakeholders raised concern about the electrification of the line and what it means for the areas surrounding the track. Respondents questioned the safety of the structures needed for the electrification infrastructure, including their size and placement. Respondents questioned the strength of the electricity supply, whether there was a backup supply, and the safety implications of high voltage overhead lines adjacent to the tracks and crossing the rail line. The use of electricity “so close to residential properties” was also noted as a concern for stakeholders. Some expressed their concern about exposure to electromagnetic radiation which “is already a concern due to an existing high voltage electric pylon running alongside the track”.

Stakeholders asked the impacts of electrification as well as the clearance required for the overhead lines should be specified and shown clearly. In addition, for any location where the mitigation measures required for safe operation, impacts the public domain and necessitate any lateral clearance requirements or measures to prevent any interference with the overhead lines, these should be specifically noted and shown to stakeholders.

4.2.9. Human Health

Stakeholders expressed concern that the proposed works will have an impact on their health. It was reported that current maintenance work occurs at night along the track, which is causing severe sleep disruption which many respondents fear will continue, if not worsen, with the DART+ SW project.

Stakeholders noted concern about the rise in daytime noise levels during and post-construction, which they believe will interfere with their quality of their life. Stakeholders believe these proposed works may permanently “impact their lives in a negative way” including making them “anxious, stressed and frustrated”. Some stakeholders also expressed concerns regarding the electrification of the line and the “exposure to electromagnetic radiation”.

However, some respondents suggested that the proposed works will alleviate any overcrowding and “crushing” during commutes on the line, improving the quality of the journey’s rail users have.

4.2.10. Hydrology & Hydrogeology

Stakeholders suggested that surface water management / water retention should be given appropriate consideration at this early design stage. Respondents commented that surface water should be managed so that “discharge to public sewers is avoided whenever possible” and so “does not impact on the local areas flood relief

scheme”. Respondents requested that flood risk identification be done, and prevention measures should be maintained to standards under the Arterial Drainage Acts of 1945-1955. Stakeholders also noted that there are two drains crossing the tracks which will need to be considered / replaced during the works.

Stakeholders were unsure about the proposed site for attenuation facilities as the identified location would mean a loss of green space for residents. It was suggested that the car park in the Inchicore Works Estate “could be a more suitable location for these facilities”. Respondents suggested that appropriate planning and assessment of the railway must take place, including possible flood risk areas such as those surrounding the substations, as they could affect the rail line if flooded.

Respondents noted that “drainage requirements” may be needed i.e., Sustainable Drainage Systems (SUDS) to be implemented in the management of surface water. Stakeholders noted that there are restrictions on the “construction, replacement or alteration of bridges and culverts over any watercourse”, and that appropriate consent from the Commissioners would be required under Section 50 of the Arterial Drainage Act 1945. They also noted that flood zoning guidelines from Local Authorities should also be considered.

Stakeholders outlined that they are unsure of the drainage provision as set out in Annex-3-6 Technical Optioneering Report sections 2.9 and 5.7.

4.2.11. Interactions and Cumulative Impacts

Submissions received requested that adequate rodent control and management measures are put in place to ensure that the proposed works do not have cumulative impacts on the surrounding communities, as works have the potential to disturb rodent populations.

The cumulative impacts of construction and the increase in trains was noted by stakeholders. They questioned the possible cumulative impact both factors will have on air pollution in the area.

4.2.12. Land and Soils

Stakeholders expressed an interest in the preservation of marginal areas such as between walls and road for grassland and trees.

Submissions expressed concerns that the impact of the proposed works could negatively impact the embankments and their stability, which could ultimately affect roads or buildings surrounding them. Further submissions received about the impacts of construction and the movement of soil that it could disturb the rodent population and cause problems for the local communities. A full Environmental Impact Assessment will ensure stakeholders concerns are alleviated.

Concerns were raised by respondents about the effects the climate change may have on the locality and the proposed works. The possibility of high rainfall/flooding may impact the works in the future and should be planned for appropriately. Respondents stated that the removal of land from their gardens to cater for the expansion to four tracks is not acceptable and should be avoided or compensation given. Submissions stated that as more people work from home green spaces and gardens have become a priority.

Stakeholders stated that as the local community was being impacted to a large degree for the proposed works, that the design process engage with the locality to consider giving them a piece of derelict land that is situated behind their clubhouse on Fassauga Avenue.

4.2.13. Landscape and Visual

Stakeholders noted that the protected structures along the proposed route are also part of the visual scenery and should be left where possible.

Submissions noted that the recent removal of trees along the proposed route has impacted on increased noise levels, but also make for good visual / privacy barriers to the tracks.

Submissions noted their concerns over the possible loss of green spaces, mentioning that they spaces are few in number. To mitigate against this, it is preferred that any left-over space or border spaces are planted with grass or trees to keep the landscape visually appealing. Stakeholders believe that the design process is key to ensuring that the locality is not adversely affected and remains, so far as applicable, the same. Some submissions noted that the greenery either side of the track / tunnels helps to ease what residents can see of concrete / trains and should be continued in the proposed works.

Submissions were received that expressed an issue with the proposed alterations to Le Fanu Bridge, they noted that large amounts of cement are not visually appealing and means the removal of green spaces and trees. They note that pre-casting cement might be cheaper and suitable for motorways but should be reconsidered for residential areas. Submissions requested good quality, nicely designed walls and fences as replacements. It was stated that the footbridge can be seen from the residential areas and any alternative that prevents further blocking of the view should be taken.

The overhead electrical line required for the proposed line was mentioned by submissions as having a visual impact on the area and identifying the location of the portal structures is difficult as it is not clear from the technical drawings.

4.2.14. Material Assets

Stakeholders' submissions wished it to be known that any assets that will be affected should be given appropriate consideration, to ensure the area and local community do not get adversely impacted.

Submissions stated that compulsory purchase orders or infringements on property, infringes on people's rights.

Submissions noted the various businesses in the industrial estate that may be affected by the works and the extent to which access may be restricted during the construction works.

Stakeholders noted that currently without more stations, residential properties will be affected more than benefitted. It was suggested that it will increase traffic and noise in their area, which will affect their quality of life. They wish for any works to maintain the green areas they have and for proper design to be considered if they must be affected.

Submissions noted that there are several pieces of existing infrastructure which cross the railway line, including the existing gas pipeline from Grange Castle. They note it important to ensure that any additional utility links or upgrades across the railway line are constructed prior to the electrification of the route.

4.2.15. Noise and Vibration

Stakeholders highlighted concern with regards to the proposed works and their impact on noise levels for residents. They stated that not only will the construction phase of the project impact them but also post construction as the works will mean an increase in the frequency of passing trains. Stakeholders argued that the trains often pass at high speeds, 'honk their horns early in the morning or late at night' and idle noisily on the tracks. Submissions questioned if horn use exiting tunnels was essential during, early morning / late night trains or if another warning system could be used, as this is expected to increase with the proposed frequency improvements.

The vibrations of passing trains was also an ongoing concern for residents. They stated that often during peak times that their properties glassware rattles in the cupboard, something they fear will only increase with the increase in train frequency. They fear this affects the structural integrity of their buildings. Although submitters did note that the change to electrified trains should reduce noise levels, it will still produce noise, and with

increased services will remain or exceed current levels. Submissions noted that noise levels in the current information is measured in decibels and state that little consideration has been given to the emitted frequency of rail noise, its intermittent nature or associated vibration, all of which add to the rail noise issue. It was noted that vibrations caused by trains, can disrupt local businesses citing a technical products assembly plant in the area.

Respondents noted that current maintenance works on the tracks, which often occur in the early hours of the morning is essential but can be loud and disrupts their sleep. They believe that steps can be taken to minimise the noise at such late hours and that adequate notice be given.

The issue of compensation to residents in the areas affected was raised by stakeholders. They believe that with the increase in trains and noise that the proposed works will bring, that they should be entitled to seek redress to upgrade their noise mitigations such as triple glazing windows and thickened doors.

Stakeholders stated that mitigation measures should be designed and implemented along the track and particularly where in residential areas where the impact will be greater. Stakeholder statements raised the issue of passenger noise. They believe that the increase in passenger numbers will result in a rise in noise levels around stations.

Submissions stated that planting high density trees and shrubs can significantly reduce noise. The effectiveness of noise reduction is closely related to the density of stems, leaves and branches. Noting that noise is more effectively reduced if the noise barriers (like trees) are close to the source.

4.2.16. Population

Stakeholders outlined concern that the proposed works and stations will not allow for optimal access for the population areas that need access. They noted population levels in the areas where the works are due to take place are set to rise considerably in the next few years and the line should allow for this expansion. As the populations rise, the line will be used considerably more, which should be considered in the design process and include more stations to allow for future demand. Submissions stated that more stations would mean more access for users which in turn could result in more fare revenue and a higher turnover for Iarnród Éireann. Submissions noted that feasibility studies should take place to ensure use, access and areas are not overlooked. They noted that the proposed number of stations is severely minimal in comparison to other DART developments such as the Coastal route. According to respondents this route has a station frequency of one per 1.06km of track. Respondents believe this should be replicated along the DART+ South West route. The electrification of the line is key to achieving this according to submissions, as it allows for increased frequency and cuts journey times which is key to attracting users.

Stakeholders stated that 'future proofing' the line is key to ensure that developments are appropriate to the estimated usage and population in years to come. Submissions referred to the DART+ expansion as a key strategic infrastructure for the development of the Greater Dublin Area, and to help Ireland achieve its climate action goals. Submissions stated that the current plans impact the locals around the works more than it would benefit them, which is unacceptable.

Respondents noted their support for the works as it would alleviate car congestion and encourage them to use the line, however they wish for it to proceed at the same rate as the housing growth in the area around the line.

According to stakeholders, the proposed works do not serve the growing populations of Kildare and a preference was indicated for the populations of the large towns of Kildare to be considered as an integral part of the project.

4.2.17. Traffic and Transportation

Stakeholders noted the importance of the project for traffic congestion and provision of transport options. The proposed works and developments, according to respondents will enable a large portion of people who typically

use single occupancy vehicles to make the switch to mass transport. Stakeholders noted that the electrification of the line and increased frequency of service would allow them more options when commuting to work and for social reasons.

Submissions mention that one factor that concerns them is that, in the short term, the developments will impact current rail services and will push rail users back to private transport. Submissions suggested that the proposed works would be used even more if further transport options are available near / at stations, such as stops for BusConnect, access to LUAS lines, Bike rental / parking and pedestrian facilities. This integration, as stakeholders refer to it as, would increase the options commuters have when using mass transport and would enable more complex journeys to take place, which in turn would entice more users. They noted that this is currently not the case but expressed the hope it would be.

Submissions noted the benefits to the environment that the proposed works will have, as they believe with more stations and increased services that more commuters will favour the train over private cars, cutting down on emissions and traffic congestion.

The impact of the works on traffic around the proposed line concerns stakeholders. They believe that the works will slow what is already congested traffic around the Greater Dublin Area. They wish for low impact on traffic during the works. Submissions noted that some traffic management measures around the track will need to be considered for improvement to minimise disruption to traffic. Respondents wish to be assured that vehicle access to private property will not be affected during any works in their vicinity.

Respondents do note that feasibility, traffic and video studies should take place to minimise impact of traffic, access to residential properties and businesses which will help maximise the potential good that may come from the project. They stated that any traffic changes / access in the areas effected should be notified in advance.

4.3. Stations

Stakeholders suggested various design elements for stations including level / step-free access provided on both sides to maximise accessibility and minimise walking distance to the stations, ramps, multiple exits to maximise local access, and an open plan design without turnstile barriers to reduce station size and improve passenger flows. Further station design suggestions included the provision of well lit, covered and secure bike parking and provision for bus stops with appropriate parking bays and shelters to be provided at / adjacent to all stations where this is feasible. Other feedback stated that pedestrian crossings should be raised, continuous, and located at desire lines.

Feedback was also provided in respect of existing stations and possible further station locations within the project extent, and this is detailed below.

It should be noted that assessing the feasibility of a potential station at Heuston West has always been included in the scope of this project, the inclusion of any additional / further stations to the line is currently beyond the scope of the project.

4.3.1. Adamstown

Submissions stated that the increased services and associated train capacity provided by the project will greatly assist the promotion of public transport for the current and future residents of Adamstown. Furthermore, it was suggested that appropriate pedestrian access into Adamstown rail station is an issue and needs to be upgraded to ensure safe and comfortable access to the station building for the residents of Adamstown.

Stakeholders queried if the station in Adamstown would be affected by the DART+ SW works.

4.3.2. Ballyfermot

Stakeholders cited a need for a station in Ballyfermot. Feedback stated that infrastructure is already in place for a station in Ballyfermot and that a large number of commuters are present in the area, who would utilise the station. Ballyfermot was also cited as having the potential to become ‘extremely popular’ once Park West and Glasnevin stations are built and the area was described as being located in a well-established suburban area with excellent regeneration opportunities.

However, the gap between the two stations of Park West and Glasnevin was described in the feedback as being ‘too far apart’ and missing out on entire communities in between.

Submissions cited it was ‘unfair’ that local communities would have to deal with the construction of the railway line but not receive any benefit from the line. Feedback stated that Ballyfermot has a high residential population who are in need of a station.

In terms of other transport options, feedback stated that the LUAS station was too far from the village, there are no other transport options in the area and no LUAS stop. The new BusConnects service was cited to remove a main bus line in the area which will further decrease transport options.

Feedback stated that station infrastructure for Ballyfermot should be built now and not in the future and by doing this, the project would be more cost effective and ensure more local support for the project.

Submissions cited that stakeholders are ‘keen to see community gain within the area’ and to contribute towards the project’s goal of transport emissions reductions.

Feedback described an Orchard in the Ballyfermot area that ‘should not be disturbed’ as it is a community initiative by several residents, as well as being an area of great biodiversity which is beneficial to the residents and wildlife.

4.3.3. Cabra

Feedback noted that in earlier publications about DART+, a new station in the Cabra area was proposed which has now ‘disappeared’ from the more recent documents and is a ‘missed opportunity’. It was suggested that a feasibility study should be completed to assess a potential station in the Cabra area.

Submissions outlined the need for a station in Cabra providing several reasons. These included the rising population of ‘25,000 people in the area’ who need interconnected transport links to and from the city, as a result of the many apartment complexes in the area, the large housing development currently under construction along the railway line and the recent location of TU Dublin in Cabra.

A station in Cabra was described in the feedback as ‘not just desirable, but a necessity’, as Cabra is a rapidly expanding suburb which seeks to reduce private vehicles and encourage public transport, walkways and cycle routes. Apart from the obvious business, cultural and social benefits, in terms of climate action, the addition of a Cabra stop was cited to reduce carbon emissions by taking cars off the roads and being in line with Cabra’s local Sustainable Energy Community (SEC).

Regarding traffic and transport options, feedback cited the increased traffic coming through roads in Cabra and the need for other transport options to take cars off the road, as buses are ‘being drastically reduced with BusConnects as main bus routes serving communities are changing. However, stakeholders also cited BusConnects as a potential transport link to integrate with a station in Cabra and make the Greater Dublin Area more accessible to serve people working and commuting within it. Stakeholders noted the extended LUAS line which could be expected to be even busier than the current line and a Cabra station would get the most out of the expansion of the DART line. Furthermore, a lack of public transport options in Cabra was cited as an additional need for a DART station in the area.

A station in Cabra was cited by stakeholders as needed to provide increased connectivity between Heuston, Parkwest, Celbridge, Glasnevin and connecting with Metrolink as far as Swords and DART+ West between Maynooth and Connolly / Spencer Dock. Further interconnectivity options cited in feedback included connecting to the LUAS Green Line and the existing LUAS stop in Cabra.

Suggestions for the location of a station in Cabra included Marlborough Road which would link with Glasnevin and the Phoenix Park Tunnel Branch Line. Satellite data was cited to clearly show a large area that could accommodate a DART+ station. Feedback cited a currently available site which was 'set aside a number of years ago for such a scenario' and was previously a passenger rail station up until the early 1950s called Cabra Bank.

Submissions noted a need for the provision of stations within the M50 to provide commuting choices for passengers in areas such as Cabra. Feedback stated that the station could be designed with stairs and platforms rather than island platforms.

Stakeholders cited that Cabra residents would have to endure the construction of expanding bridges and tracks in the area, as well as noise and vibrations of the trains, yet would not receive the benefit from the expanded DART line.

Stakeholder feedback noted the understanding that construction and opening of a Cabra station cannot be delivered within the timeline of the programme, but that the planning process needs to start now rather than after the project. In contrast, some stakeholder feedback stated that construction of Cabra station should happen now while works are underway.

Suggestions for a stop in Carnlough Road were cited by submissions, due to 'the Broombridge stop and other stops being too far away to service many households in the area, especially around Cabra'.

It was suggested that a space for a station in Carnlough Road be preserved and feedback suggested asking developers in the area to contribute to the cost of construction of the station, as 'it would add value to the homes'.

Submission feedback cited the need for a station at McKee Barracks. A vacant strip of land beside the Barracks, which could act as an entrance to the station was cited in the feedback. McKee barracks was also noted in the feedback to likely be developed in the future as an architecturally and historically unique part of the city and therefore a station in the area would be a great asset to the many locals and open-up the area for future high-quality developments such as hotels, education and recreational facilities.

4.3.4. Hazelhatch

The Celbridge / Hazelhatch station was cited in the feedback as being located between towns, with no safe cycling infrastructure, no pedestrian crossing infrastructure and is designed for park-and-ride users, which is 'in contradiction with modern planning concepts, and is overdue a review'. Stakeholders expressed a need for walking and cycling infrastructure to be put in place to access the station. Feedback also stated that a full weekend service to Grand Canal Dock from the Hazelhatch station was needed.

Other feedback stated that the extension of the DART line to Hazelhatch would be a vital contribution to the infrastructure and would add to the development and accessibility of Celbridge. Stakeholders cited that increased services and associated train capacity provided by the project will assist with the promotion of public transport for the current and future residents of Celbridge and that anything that improves the capacity and frequency of train services from Hazelhatch will have a positive impact on commuters from Celbridge.

Future suggestions stated in the feedback included that the proposed line should be developed beyond Hazelhatch to allow for future development of areas.

A submission by owners of landholdings in the area stated that they can accommodate local road development to make Hazelhatch station more accessible to the people of Celbridge, thereby encouraging the use of public transport.

4.3.5. Heuston West

Stakeholders welcomed the proposition of a potential station at Heuston West, citing the increased interconnectivity it would bring to the wider transport network in the city and the surrounding areas.

Submissions cited that Heuston West should be in the scope of this project and not just a feasibility study as connections to Kilmainham / Islandbridge South Circular Road junction should be a priority.

Stakeholders stated they were interested in the proposed Heuston West station but were disappointed to see a lack of overall information available at this stage of the project.

The challenges of a station in Heuston West were expressed by stakeholders and feedback stated that integrating the station with the surrounding local areas may present a challenge due to the proposed location. It was noted that access to such a station should be via private apartment complexes but that all access points must be publicly accessible. Feedback suggested placing a station underneath the South Circular Road junction to provide significant connectivity options with the local area, which is currently poorly served, and that this should be considered, as major works are already needed at this location.

Feedback raised the question of how the project's proposals for Heuston West would integrate with proposals for the DART Underground. Feedback further suggested that the station be called 'Islandbridge' instead of Heuston West to be more inclusive of the community where it is situated.

4.3.6. Inchicore

Stakeholders cited that infrastructure was already in place for a station in Inchicore and to not include a station here would be 'a massive oversight'.

Feedback cited a large number of commuters and a growing population in the area who need a station in Inchicore as well as the communities in the area who will be 'subject to the disruption of the line works but won't benefit from the improved train service'.

Furthermore, the addition of a station was cited as increasing fare revenue and maximising climate change mitigation.

Feedback stated that the largest amount of land owned by Iarnród Éireann inside the M50 is probably at the Inchicore works, making it a suitable place for a station.

The proposed BusConnects service was cited to remove an important bus route in the area leaving residents with less transport options and encouraging more driving.

The need for a station on Jamestown Road was outlined, due to the large industrial estate present in the area. Stakeholders cited that Jamestown Road Industrial Estate 'has been rezoned from industrial to housing' and that 'it makes sense' to plan a station nearby to service the many apartment blocks in the area.

4.3.7. Kilmainham & Islandbridge

Feedback cited that the station at Heuston West should be named 'Islandbridge' to reflect the nearest suburb which is closer than the main Heuston entrance. A station in Islandbridge itself was cited as being needed in the area to service the local people living there.

The need for a station in Kilmainham was described in the feedback. Stakeholders expressed concerns that there are 'huge populations and commuters in Dublin 8 and Dublin 10 that are not served in any way by this proposed

plan'. Similar feedback stated that it is 'frustrating' that the DART line goes through communities in the area 'and have run since the lines were laid more than 150 years ago' but that there are no stops for local people to use. Stakeholders cited that communities in the area 'will bear the brunt of the works without any community gain'.

Feedback cited a need for a stop in Kilmainham to encourage less car use in the area and allow for greater connectivity within the city. A DART station in Kilmainham was cited to bring benefits for '14,000-15,000 local residents' who would be able to access employment, leisure and education facilities along the DART network.

Tourism, particularly around Kilmainham Jail, was cited as an important reason to include a station here.

4.3.8. Kildare Town

Stakeholders expressed a need for a station in Kildare Town in order to provide access to the DART+ network for all of Kildare's major towns.

Feedback described the Kildare Route Project, which was undertaken a number of years ago to examine and scope a four-track line to Kildare Town as part of exploratory work in order to increase the capacity of the line in Kildare; stakeholders asked for this project to be reviewed again.

4.3.9. Kishoge

Feedback noted the Kishoge station citing that it has not been in use since its completion in 2009 and should be incorporated into the DART+ Programme. The opening of Kishoge was cited to be an important priority and a phasing requirement of the SDZ Planning Scheme and therefore should be recognised as part of DART+ South West.

Stakeholders cited the need for a station in Kishoge due to its interface with the surrounding urban core development and the need for more frequent and reliable services with an operational Kishoge station. Submissions expressed that bus services in the area are not suitable for commuters due to the indirect routes and that a Kishoge station would vastly improve this situation and encourage more people to use public transport rather than driving to the city centre.

Stakeholders noted a new car park recently completed for the station 'which means little rail or road infrastructure would be required to open the station.'

4.3.10. Kylemore

The need for a station in Kylemore was highlighted by submissions due to large industrial and retail trading estates. It was suggested that large populations of commuters in the Kylemore area would benefit from a station within the DART+ SW programme.

Queries regarding a station at Kylemore were voiced by respondents, including; 'Why is a Kylemore station not included in the initial proposal?'; 'If the bridge is being widened, why can't a station at Kylemore be constructed at the same time?'; 'What is needed to get the Kylemore station included in the current proposal?' and 'What is the timeframe for the construction of a Kylemore station in the future?'

It was suggested that the Kylemore station be constructed as 'an island' rather than two separate platforms to save costs. Furthermore, feedback suggested that by constructing the station within this phase of the project, it could simplify the overall process and reduce overall costs.

The lack of a Kylemore station in the scope of the project was cited as showing 'a real lack of consideration for the community' and it was 'disingenuous' to say a station may be considered at some point in the future when infrastructure should be considered now as part of concrete plans. Stakeholders cited frustration that the DART would be travelling through the Kylemore area but not stopping there, and so not providing benefit to local residents. Furthermore, submissions cited the 'unfairness of local communities having to deal with the disruptions

of the development yet reap none of the benefits'. Stakeholders felt 'overlooked' by the decision to not include a Kylemore station in the project scope and felt there is no reasoning as to why the station is not included as part of the current proposal.

Feedback stated that it is imperative that the option for a station in Kylemore is investigated now in order for the development to be built right the first-time round, making reference to the failed M50 and Red Cow developments.

Submissions further stated the need for a Kylemore station, citing that it will be 'an essential ingredient' to create significant development in the area and would connect the outer suburbs with the Grand Canal Greenway and Red Line LUAS. The area was expressed by submissions to have limited transport options for such a high-density population area, and so a DART station would be highly utilised in the area and is needed to meet future demand. Feedback stated that not building a Kylemore station would be a missed opportunity and that the current focus of the project is too narrow.

Stakeholders noted that extra stations such as in Kylemore are needed to serve as many customers as possible, attract workers to the area, service high population growth and provide extra finance coming in from ticket sales. Other reasons included creating a better quality of life for residents and contributing to climate action.

4.3.11. Naas / Sallins and Newbridge

The need for a DART service to Naas / Sallins was expressed by stakeholders.

In the feedback, reasons for a station in Naas / Sallins included the surrounding areas and townlands experiencing current and future levels of residential development as part of the Greater Dublin Area, the need to provide transport infrastructure for increasing populations and the failure of current transport infrastructure to facilitate travel requirements of residents in the area.

Further feedback on lack of current transport options cited that Naas / Sallins and Newbridge train stations currently have fewer travel alternatives/options as compared to the stations from Hazelhatch into Dublin. 'Major overcrowding' at Naas / Sallins on city centre services at peak time hours was noted in the feedback, which was cited to impact the safety of commuters and highlighted the need for increased public transport capacity. The area was described as a booming commuter population as 'the largest commuter town in Co Kildare' and the Eircode with 'the most housing completions in the country for four years in a row'. The need for 'a high-frequency rail service like the rest of the commuter belt' was cited. A reply was attached in the submissions to the recent Parliamentary Question indicating the growth levels of passenger usage in both Naas / Sallins and Newbridge train stations which showed with current population levels there has been a year on year increase of passengers using these stations which will grow considerably again with the introduction of a DART+ to these towns. Feedback stated that figures illustrate very clearly that the busiest and most used train stations on this Kildare line are Naas / Sallins and Newbridge with nearly 4,000 commuters using these stations daily.

Extending the DART+ line to Naas / Sallins and Newbridge is needed according to feedback, to reduce the number of commuters leaving these areas and using the services directly from Hazelhatch Train station. An extended line was also cited to reduce the number of cars traveling from the surrounding areas and greatly easing peak time traffic on the key primary and secondary roads in these areas.

Submissions cited that Deputy James Lawless T.D. has made numerous representations and has held regular meetings over the last four years with Iarnród Éireann, Bus Eireann, the Minister for Transport and the NTA specifically raising transport capacity problems in Naas / Sallins / Newbridge and the surrounding areas. The current DART+ SW programme was cited to do nothing to alleviate traffic on 'Ireland's second busiest road', the N7, and 'does very little for commuters outside the suburbs particularly Kildare residents'.

Stakeholders further stated that extending the line to Naas / Sallins will satisfy the appraisal criteria set out in the Common Appraisal Framework namely; economy, safety, integration, environment, accessibility and social inclusion.

Further reasons cited to extend the DART+ line to Naas / Sallins included allowing more people to make sustainable travel choices and will contribute to a reduction in emissions within Dublin while providing a reliable alternative to private cars.

Stakeholders stated that the Kildare Route Project undertaken a number of years ago, which examined and scoped a four-track line to Kildare Town, had undertaken a considerable amount of exploratory work on the option of increasing the capacity of this line in Kildare and this work should be reviewed and used in extending the DART + to Naas / Sallins and Newbridge train stations.

In terms of infrastructure and investment, feedback cited that a DART+ line to Naas / Sallins and Newbridge will respond to network constraints and increases in demand by utilising existing infrastructure and developing additional interchanges with other public transport modes. The population and traffic volumes in Dublin and the surrounding counties was cited in feedback to be growing rapidly and thus investment is needed in public transport to sustain economic and population growth around the Greater Dublin Area. Stakeholders expressed understanding that 'there are issues with the size of the bridges on the current line, given this line is using the IE 22000 Class trains to get past this limitation however in the long-term these bridges should be bypassed with a new line'.

Stakeholders expressed that they felt Naas / Sallins had been excluded in all major infrastructure projects including both the Dublin Suburban Strategic Review and the Platform for Change Integrated Transportation Strategy for the Greater Dublin Area. Feedback stated that the Greater Dublin Area should be upgraded on a continual basis in proportion to population sizes and densities 'as is the European norm'.

4.3.12. Park West & Cherry Orchard

Feedback expressed disappointment at the location of the proposed Park West / Cherry Orchard station as it was deemed too far away to be used by commuters in the area. Suggestions of a different location were cited, including a pedestrian way from the old Clondalkin station at the Station Road bridge along the railway line into Park West in order to shorten the route for residents of Cappamore, Palmerstown Woods and Neilstown to access the station, as well as making Park West more 'disabled-friendly' for wheelchairs.

Submissions noted the need for additional stations between Heuston and Park West.

Stakeholders cited that none of the proposed track modifications between Hazelhatch and Park West would be required if DART Underground proceeds first.

4.3.13. Phoenix Park

Feedback stated a need for a station in Phoenix Park, as an area that needs more options for sustainable travel. Tourism was cited as an important reason to have a station in Phoenix Park as well as increased access to the Dublin Zoo and to open the Park to rest of the city.

Submissions outlined a need for a station in this area to future proof the network for the DART Tunnel project. Connectivity was cited as an important reason in order to create a 'functioning rail line'. A query about the potential of constructing an underground station in the Phoenix Park was asked in feedback.

4.3.14. Phibsborough / Glasnevin

A station at Cross Guns was cited as a need for residents in the area. Feedback suggested a shared stop between DART and LUAS in order to ensure the network becomes integrated and provides more travel solutions for both networks.

A new station at Glasnevin was cited in the feedback to enable easy interconnections between the proposed DART+ West and South West routes by offering increased utility of the line.

Stakeholders welcomed the idea of connecting the DART line through the Glasnevin Junction as the junction 'has the potential for a high quality interlink between modes of transport for the western part of the city in the future'.

4.3.15. Croke Park / Ballybough Road

A station in Ballybough Road / North Docklands was cited in the feedback as a much-needed station in the project. Reasons for a station here include that the area is 'less well off' and 'in need of stations not only for transport but for a vote of confidence in the area'. A station in Ballybough Road was cited as being 'really important on many levels'.

Feedback expressed concern that 'an urban train line cannot be built without stations'. Further feedback cited that it is 'inconceivable' that there is only one station proposed in the entire city and is not easily accessible by existing residential areas. Stakeholders cited that more stations were needed throughout the city and Croke Park should be included as a station to service the crowds who use the park, as well as the residents in the area.

4.4. Project Benefits

4.4.1. Accessibility

Submissions noted the need for stations to be designed with better consideration/integration with all other transport modes be this walking, cycling, integration with buses, car park and ride etc. It was suggested that better bicycle storage at stations or capacity for bikes on trains would be a major benefit for improving accessibility, allowing passengers to forgo private car use.

Concerns were expressed that accessibility is severely limited due to the distance that train stations are away from certain areas and that no new stations are planned for these areas as part of this overall proposal. Indeed, submissions also cited the need to further expanded the project to Kildare to gives these commuter towns more accessibility to public transport considering their growing communities.

However, submissions received did cite their support for the improvements in public transport and the benefits it would provide in terms of accessibility as it would enhance greater public transport opportunities for work, education and leisure. Highlighted was also the general need for improvements in public transport to freely move around Dublin City.

With regards to bridge designs, it was noted that their design should provide for segregated walking and cycling facilities with further queries if certain bridges would be accessible to wheelchairs. Furthermore, it was cited that the Khyber Pass Footbridge should be opened to the public which would allow for greater connection with the Red Line LUAS and other services on Tyrconnell road.

The proposed works were noted by stakeholders as having a positive impact on the environment by encouraging road users to use the train if it is accessible to them, however increasing the road width for cars surrounding the track should be avoided.

To ensure that the proposed developments are climate friendly, accessibility for wheelchair users and cyclists would be of benefit, according to submissions.

4.4.2. Climate

Submissions cited that increased electrification and extension of the DART line to the South-West would help reduce car journeys and as a result help reduce congestion and pollution caused by single-occupancy vehicles. As one submission cited, 'anything that can be done to be less reliant on one's car while still being convenient has to be great and good for the environment'. However, it was noted that all aspects of the design and implementation of the project must align with empowering people to walk, cycle and take other forms of public transport. Therefore, the need for joint up intermodal transport / connectivity be this clearly marked cycle / footpaths to the station, bicycle parking, frequent bus services need to be seriously considered as part of the project to ensure its climate success.

Respondents questioned if the electricity being used was coming from "green energy sources".

Although submissions highlighted the climate benefit in Ireland's transition to a low carbon economy, submissions did note that more stations along the route were required to ensure that areas not served by stations could have a modal shift away from private car use to public transport as well.

4.4.3. Frequency / Capacity

Submissions cited the benefit of increased frequency / capacity the project would provide as it would encourage a modal change, reduce reliance on private cars and transform Dublin into a resilient city. The need for the increase in frequency / capacity was also highlighted in submissions as it would alleviate overcrowding currently experienced and would give commuters a more flexible timetable to travel. In addition, the increase in frequency / capacity would also provide a major benefit for the housing developments that are currently being built or are planned to be built in for the surrounding areas.

However, concern regarding noise levels as a result of the increases in frequency from 12 to 23 trains per hour was expressed. Submission who expressed this concern wished to know what the effect would be on them and what would be done to alleviate noise issues.

4.4.4. Future Improvement

Submissions cited the need for more stations to be added to the current proposal. It was highlighted that there is a large gap between Park West and Glasnevin without stations. As substantial works are being undertaken, it was cited that now is the time to install stations here or at least complete enabling works for them. It was also noted that although the addition of stations may somewhat reduce travel time, its benefits to users outweigh the costs. In addition, cited was the current proposal being excellent for long-distance commuters, but it needs to offer more stations between the canals to reduce car journeys. Furthermore, adding more stations will be important in maximising the opportunities for users to interchange with other modes, e.g., train, LUAS and to provide options other than cars.

Lastly, submissions noted that Iarnród Éireann should collaborate closely with TII (where appropriate) to demonstrate that the design of stations and surrounding public realm has taken cognisance of potential future development above.

Stakeholders noted that “Irish Rail’s 2040 network strategy” lists the electrification of intercity services to Cork / Galway as an objective. Some stakeholders questioned if installing head spans / portal frames spanning both tracks, would make future electrification of the tracks cheaper and easier.

4.4.5. Journey Time

Submissions cited that the project would allow them to avoid lengthy commutes by enabling them to make faster and more frequent journeys to and from Dublin City.

4.5. Consultation & Engagement

Stakeholders questioned the relevance of the information they had received. They noted that although the information leaflet had plenty of information it lacked specific detail. Extra leaflet drops regarding the public consultation was suggested to ensure all relevant bodies / stakeholders were informed.

Submissions noted that some links on the project website were mislabelled or inaccessible and did not have enough relevant detail. It was noted that this should be corrected and redistributed to stakeholders. Respondents requested detailed maps as to the location of the proposed works to ensure clarity as to which sites they manage were impacted.

Submissions stated that not enough public consultation had taken place during the consultation window and that it did not give those affected by the proposed works enough opportunity to come to an informed stance. Timely, accurate and well-presented further public consultations were noted as key to the success of the project by submission. They stated that the process of consultation would allow for informed choices and opinions to be made and without consultation, the project was certain to get negative feedback. Photomontages were requested by stakeholders at the next public consultation to give the public accurate ideas of the impacts of the works. Stakeholders noted that the times of public consultation webinars did not suit all intended to be affected. They suggested that GDPR guidelines should be followed, and the webinars recorded for the public to access.

Requests were made for the prescribed bodies to be fully / re-engaged as according to some stakeholders, not enough was done to alert them to the proposed works.

Stakeholders requested more information about the public consultation process, the deadline for submissions and the availability of information and why leaflets were delivered to those unaffected by the proposals. They questioned the cost of printing the leaflets and for this information to be relayed to them.

4.5.1. Feedback & Reports

Stakeholders expressed an interest in receiving a report on the first public consultation and for it to be widely disseminated. They believe that this will help those who will be affected to understand the outcome and submissions of the first consultation and help inform their options for the next.

Submissions noted that the project is one of the most ambitious expansions of the rail network in the Greater Dublin Area and that all findings from the Environmental Impact Assessment Report, Heritage Impact Assessments Reports and site surveys should be published for public viewing.

Stakeholders noted the lack of public engagement prior to the publication of the proposed works, noting that it was disingenuous to inform them at this late stage in the design process.

Current train lines from Dublin to Cork were sought to be electrified and if the existing signalling on the tracks allows for two-way traffic. Regarding the current Drogheda to Dundalk line. Submissions questioned why it was displayed as grey on technical maps.

4.5.2. Meeting Requests

Stakeholders acknowledged the current COVID-19 situation but stated that they would prefer face-to-face meeting over online consultation. Stakeholders noted the importance of webinars in the current COVID-19 environment and submissions noted that recordings being made available to those who cannot attend would be of benefit to those affected by the proposed works.

4.5.3. Process

Stakeholders questioned how they would be informed of the further consultations, planned or otherwise. They wished for them to be extended to other areas to fully engage with the local communities. Submissions enquired about the process of public consultations and how COVID-19 may affect or alter the process.

Stakeholders noted the importance of the public consultation process, as they stated it is key to the public becoming aware of and inputting their opinions into the design process.

Respondents questioned the consultation process and noted that they received the consultation information and leaflets but are not in the vicinity / impacted areas, they noted that should not have received the information / leaflets. Submissions furthered this point by saying the information leaflets did not have relevant information to them and questioned the process of compiling relevant information to be published.

4.5.4. Timing

Stakeholders requested to be given an accurate timeline for the construction process. They believe this will help to inform them of the impacts on them and their properties. They wished to ensure that they are told in good time of any commencement of works near their residential properties.

Submissions noted a preference to receive information about the public consultation process and its timeline to ensure that they can voice their opinions.

Respondents noted that they see the benefits to this project and wished for the timeline to be moved up, for works start as soon as possible. They fear the project will be delayed and the current traffic congestion will only get worse in the interim.

4.6. Design

Submission on designs cited the need for commitment for better intermodal integration, in particular bike-rail-bike which could be done by providing secure bike parking at each rail station. In addition, cited was the need to 'inject some fun and colour into the station designs'. Other submissions regarding station designs focused on improving accessibility by maximising general local access as well as improving stations disability-friendly features.

Submissions of concerns regarding the design cited that the project is bypassing several urban areas without providing a station i.e., Ballyfermot, Inchicore and Cabra. Submissions cited that there seems little point in developing the line and not giving people the opportunity to use the infrastructure, therefore more stations are required within the project scope.

In addition, submissions cited that expanding the project to Naas and its surrounding towns is required as these areas have a growing population. With specific regard to the Naas / Sallins station, submissions cited it is severely in need of an upgrade. Cited was that there are no proper feeder buses to the station and the car park is often overcrowded, which in turn is not encouraging people to use public transportation. However, cited was if the

station is connected to the DART, provided with adequate park and ride facilities and had bus connections, then the station would thrive.

Stakeholders noted that all aspects of the design and implementation of the project must align with the goal of reducing the use of private vehicles.

Other areas of concern noted in the submissions regarded compulsory purchase orders being made on back-gardens to widen the track, landscape and visual impact from the removal of trees as well noise impacts from the increased use of the tracks. It was cited that all noise reduction efforts need to be explored for areas that are close to homes, be this the installation of natural sound barriers or other methods.

With regards to bridge designs submissions noted that even if there is no immediate plan, designs for the new bridges should be futureproofed to be wide enough to fit possible new infrastructure such as cycle tracks and bus stops. In addition, submissions were also received regarding the Le Fanu bridge with specific queries on how much space will be required and how much closer homes will be to the bridge. Furthermore, the Khyber Pass footbridge was cited as being a good connector to Red Line LUAS and other services on Tyrconnell Road, if it was opened to the public.

Protection of the Inchicore Turret was also highlighted as a must in submissions due to its cultural significance.

Lastly, feedback received noted that assuming the new Heuston West station is located at Heuston Platform 10, then the most sensible place to have pedestrian access from Islandbridge would be via a new opening in the Clancy Quay boundary wall. It was suggested then people in Islandbridge could then walk to Heuston West through Clancy Quay.

4.7. Policy and Planning

4.7.1. Planning

Stakeholders cited that this project requires long-term planning and forward thinking. Respondents cited that this project has been poorly planned, lacks ambition and is not forward thinking. Concern was raised that by the time this project is complete, demand will outweigh availability.

Stakeholders referred to this project as “a key strategic infrastructure that will support the needs of a growing population and expanding workforce in the Eastern and Midlands Region”. It was noted in the submissions that public transport is essential for future growth of the economy. Stakeholders stated that this is a good use of existing infrastructure that will connect suburban satellite towns to the Dublin metropolitan area.

Respondents cited that this proposal is great for long distance commuters but does not benefit those living in urban areas or address issues of traffic congestion in the city centre. Feedback suggested that this project is at risk of “major public backlash”. Respondents referred to this proposal as being “Dublin-centric” and urged Iarnród Éireann to invest more funding outside of Dublin.

Submissions asked that Iarnród Éireann would provide stations along the route at Cabra, Heuston West/Island Bridge and Ballyfermot/Kylemore and would extend the project to encompass Sallins and Naas. Stakeholders cited population growth and increased population density in these areas and stated that Iarnród Éireann should build a service that meets demand.

Stakeholders sought clarity on how emerging proposals will be addressed as the DART+ SW design progresses and on what level of detail will be included in the Railway Order application.

4.7.2. National, Regional & Local Policy

Stakeholders cited that this project should fulfil the local, regional and national travel objectives. It was noted that it is “curious to suggest that this project will link good quality public transport to sustainable land use management and assist local regeneration” when obvious stations have been omitted from the proposal.

Respondents cited section 8.4, ‘Transport Investment Priorities’ from the Eastern and Midlands Regional Spatial and Economic Strategy 2019-2031 which supports the DART+ South West and outlines:

“New stations to provide interchange with bus, LUAS and Metro network including Kishoge, Heuston West, Cabra, Glasnevin, Pelletstown and Woodbrook”.

Stakeholders stated that the exclusion of stations at Kishoge, Heuston West and Cabra from this project does not comply with transport policy.

It was noted in the feedback that Hazelhatch & Celbridge, Adamstown, Kishoge, Clondalkin / Fonthill, and Park West & Cherry Orchard are located at major greenfield sites and that there is a lack of stations within existing developed neighbourhoods to offset this. Stakeholders highlighted that the National Planning Framework requires city authorities to limit greenfield sprawl to under 50% of housing growth and that it should be a responsibility of Iarnród Éireann to support this development pattern with its infrastructure.

Stakeholders noted that DART+ South West is identified as an Action under the TEN-T Connecting Europe Facility Programme (CEF) which acknowledges that the “upgrading this railway line to four electrified tracks will bridge the missing link by connecting the Cork Line and the Belfast Line through two stations in Dublin (the Hazelhatch and Connolly stations)”. It was put forward that only electrifying two tracks does not comply with this policy.

It was noted that Sustainable Drainage Systems (SuDS) should be implemented into the surface water management in this project.

Stakeholders noted that the repurposing of lands to the east of St George’s villas for attenuation facilities, goes against the zoning objectives of the Dublin City Development Plan 2016-2022.

Stakeholders noted that there are restrictions on the construction, replacement or alteration of bridges and culverts over any watercourse, and that appropriate consent from the Commissioners is required.

It was noted in the feedback that the Climate Action Bill will need to be considered in the EIAR and Railway Order applications.

Stakeholders noted that currently the junction design at the bridges must meet the minimum standards on a and noted that local residents need assurances that the safety and comfort of the pedestrian user is dramatically improved with traffic calming and pedestrian priority design.

4.7.3. Project Scope

Stations

Stakeholders questioned where the 20,000 passengers per direction would come from without additional stations on this route.

Stakeholders suggested that the development of new stations between Cherry Orchard and Glasnevin should be delivered as part of this project. Stakeholders are concerned that if they are not developed now that they will never see them. It was highlighted in the submissions that the route bypasses several urban areas, populated by c150,000 people, without providing stations to access the service. Respondents felt as though they are being deprived of access to reliable, environmentally friendly public transport and of connectivity with other parts of Dublin.

Stakeholders requested that the line be extended to serve Naas / Sallins and Newbridge. It was noted in the feedback that the other existing and proposed DART routes cross county borders and that the DART+ SW is 'measly' in comparison. Stakeholders highlighted that this project won't alleviate traffic congestion on the N7, which was cited as being Dublin's busiest commuter artery. Respondents highlighted that the 'Dublin Suburban Rail Strategy Review' recommended the electrification of train tracks as far as Naas and Sallins and that the '2001 Platform for Change' recommended four tracks from Sallins to Cherry Orchard. Stakeholders noted that these recommendations have been ignored in the proposed plans for DART+ South West.

It was noted in submissions that at present this project significantly underperforms in its goal to reduce passenger traffic, given the lack of stations on the route.

Stakeholders queried whether there would be increased local bus services to the stations and noted that it would be counterintuitive if commuters were driving to park at stations. Moreover, it was noted that by not including additional stations between Cherry Orchard and Glasnevin and by not extending the line to service Naas / Sallins and Newbridge, this project will not address the current rates of car travel and subsequent negative impacts on the climate change and traffic levels.

Pedestrian and Cycling Infrastructure

Stakeholders are keen to see community gain from this project, particularly in areas that will not benefit from the new train service. It was requested that Iarnród Éireann improve walking and cycling infrastructure along the new proposed route. Respondents suggested that this project could include a side-track walkway and cycle lane from Phoenix Park to Cabra. Respondents further requested that this project is used as an opportunity to address the pedestrian safety issues at the junction of South Circular Road and Chapelizod Bypass.

Sarsfield Road, Memorial Road, Clondalkin and Fonthill were further noted as areas that required improved pedestrian infrastructure in submissions.

Stakeholders highlighted that having a pedestrian access route through the Inchicore Works via the Khyber Pass Bridge would significantly reduce the walking time from Ballyfermot to the LUAS Green Line.

It was noted in the submissions that the underpass at Sarsfield Road bridge is inappropriately narrow and should be fixed as part of this project.

Future Developments

It was noted in the feedback that Iarnród Éireann should collaborate closely with Transport Infrastructure Ireland to ensure the design of stations and surrounding public realms take cognisance of potential future developments. Similarly, it was noted that every accommodation should be made to facilitate future expansion of the DART network, particularly the DART underground.

Stakeholders noted the importance of not seeing new or upgraded bridges and infrastructure in this project in isolation and ensuring that they are future proofed and consider other strategic public transport improvements, such as LUAS extensions and Metrolink.

It was requested that Iarnród Éireann protect public land for the development of additional stations on this route in the future.

Stakeholders questioned whether there was a plan to build an underground stop in Phoenix Park to enable access to Dublin Zoo.

Stakeholders further questioned why an interchange with the LUAS Green Line was not a feature of this project.

Other Transport Services / Projects

Stakeholders noted that connectivity between the North and South of Dublin is essential. Respondents questioned whether there are plans to build a tunnel connecting Heuston to the South City Centre in the future. Furthermore, the importance of North Dublin residents having access to the Metro Links was noted in the submissions.

It was questioned in the feedback whether the DART+ SW will have an effect on existing services with which it will share tracks.

Stakeholders requested that the Athy to Waterford line is extended to enable a more frequent service. Respondents noted that they would like to see the DART+ Coastal North extended as far as Dundalk. Respondents further stated that they would like to see the DART extended to Tullamore.

Stakeholders stated that the DART underground is a far more integrated and logical proposal. Stakeholders questioned whether there were plans for a station at Inchicore works as part of DART underground in the future. Respondents questioned why this project is commencing ahead of the Metro Links and the DART+ Coastal North. Stakeholders further questioned when the Maynooth line will be electrified.

It was noted in the submissions that more work needs to be done to expand public transport capacity along the N7/M7 corridor.

Respondents noted that they would like to see improved access to St James' Hospital using public transport. Additionally, respondents noted that they would like to see improved public transport in Blanchardstown.

Stakeholders noted that the proposed BusConnects route through Ballyfermot lower would remove the current 79/79a bus route and leave the area with only the new route, which would encourage more car travel.

Stakeholders questioned whether Iarnród Éireann will ever build new lines.

Connectivity

Stakeholders stated that “an integrated and collaborative approach is required to leverage optimal and sustainable improvements” and requested that collaborate with BusConnects, the Office of Public Works, Dublin City Council and the National Transport Authority.

Stakeholders questioned whether BusConnects plans had been finalised and if there would be feeder routes connecting the DART to the LUAS.

4.7.4. Project Cost & Funding

Investment in public transport was welcomed in the submissions and stakeholders felt that this project should not be scaled back for cost reasons. However, stakeholders questioned why so much public money was being spent on a route that serves so few people and that extending the route to Kildare and adding further stations would make the project better value for money. It was felt that the provision of additional stations would have a positive effect on a cost-benefit analysis.

Stakeholders questioned whether it would be more cost effective to build new stations in conjunction with works being done to bridges in this project. It was noted that stations could be built at low cost if they were designed similar to current LUAS stops. Additionally, stakeholders suggested that allocating just €45m of the programme's €2.7 billion budget would cover the cost of three new stations.

Stakeholders expressed concerns that the project may run over budget and that the budget provided is insufficient. It was suggested that feedback from this consultation be used by Iarnród Éireann to make a case for sufficient funds to build further stations that would serve more people.

Submissions outlined that Dublin's integrated public transport system is insufficiently funded. It was suggested that unused Iarnród Éireann lands be sold to fund stations along the route.

4.8. Construction

Stakeholders referred to works for the development of the scheme as a potential nuisance locally. Stakeholders noted concerns about the construction works disrupting businesses and access to business premises. Respondents asked for more information on the impact during construction and after construction, on their day-to-day lives. Stakeholders noted the cumulative impact of several works being undertaken simultaneously. The cumulative impact of transport projects (i.e., MetroLink, BusConnect and other developments close to the rail corridor) was specifically cited in the feedback.

Stakeholders sought that action would be taken to mitigate any negative impacts on local residents. Respondents sought more information on the mitigation measures that will be put in place to deal with the increased vibrations, dust and noise during construction.

Submissions outlined that previous works involved “lights being shone in bedroom windows” and “workers shouting across large distances to each other”.

4.8.1. Noise

Stakeholders are concerned about noise levels and acoustic disturbance during the construction of the project. Respondents specifically noted the noise resulting from truck’s horns, construction workers voices and loud drilling and welding at night. Submissions queried whether a sound barrier will be erected. Residents living near the track highlighted that they currently experience disruption due to frequent, ongoing maintenance works and raised concerns that the expansion of the tracks will result in an increased need for maintenance works and thus, increased disturbance. Stakeholders questioned whether a noise management system was going to be put in place during construction.

Stakeholders cited that no information regarding noise levels or systems being implemented to decrease this was made available to them. It was requested that all sound and vibration reduction solutions be considered. Stakeholders are concerned that the ongoing increased noise levels and acoustic disturbance will affect their property’s value and attractiveness to future potential tenants. It was requested that Iarnród Éireann ensure ‘appropriate robust screening measures be implemented and perpetually maintained’, that noise monitoring be carried out and that the current baseline noise levels in the area are established. The following suggestions to mitigate acoustic disturbance were outlined in the submissions:

- “A soundproof canopy as an extension of the new bridge on the South Circular Road to cover all track lines in front of the Old Chocolate Factory creating in effect an extension of the existing tunnel under the bridge”
- “A soundproof and vibration proof barrier on the retaining wall along the whole length wall at the Old Chocolate Factory”
- “Composite track implementation, anti-vibration mats, anti-vibration blankets, insulating chambers, under sleeper pads, silent track tuned rail dampers”
- Track silencing, which was noted as being the norm in residential areas in Germany and other European countries
- Sound barriers on the walls or boundary of properties
- Sound dampeners
- A cut and cover approach for the section around Heuston to Memorial Bridge to alleviate noise pollution

- A podium slab for all tracks with soundproofing of the tunnel
- Planting high density trees and shrubs
- A cover over structure in the Kilmainham area

4.8.2. Nightworks

Concern was raised over nightworks and the health risks associated with disturbed sleep. Stakeholders suggested that works on the development would ideally take place during business hours to minimise disruptions. Stakeholders stated that railway night works have “been going on for such a long time” and that there is a lack of consideration for residents living near the railway. Stakeholders outlined that “very irregular advance warning” is currently given for track maintenance, which predominately takes place at night. The noise from nightworks was described as “noticeable, constant and in some cases louder than the trains themselves”. Stakeholders are concerned about the frequency of night works increasing both during the construction and for future maintenance works. Stakeholders requested an indication of the frequency of track maintenance after construction.

Residents near the CIE Inchicore Depot queried current night-time works at the depot causing light to be shone into their home and stated that they were not informed about these works.

4.8.3. Residential Property

Stakeholders raised geotechnical concerns and questioned whether their properties would be affected structurally or foundationally during or after the proposed works. Residents are concerned that repetitive vibrations caused by trains passing by will have a major impact on the foundations of their buildings. Respondents highlighted that they currently experience vibrations and tremors caused by passing freight trains, fast trains and slow trains. It was highlighted in the submissions that the community garden in Seven Oaks supports the railway and the development’s boundary wall and currently absorbs vibrations from passing trains. Concern was raised that removing it would raise the impact of the vibrations on nearby properties.

Stakeholders sought clarity on what surrounding residential properties will be destroyed. It was queried how much, if any, of residential gardens would be taken during the construction. Stakeholders noted that they were informed of potential temporary impacts to their property and questioned what this may be. It was noted that changes in design could cause permanent impact to residential property, which would be unfavourable and met with reluctance to agree. A map of affected areas detailing the exact nature of building disruption was requested in the feedback.

It was requested in the submissions that consideration be given to ensuring vibrations are kept to a minimum. Stakeholders sought assurances around the structural integrity of their buildings, queried how Iarnród Éireann will protect their homes from subsidence and vibrations and requested more direct engineering consultations with residents along the tracks.

4.8.4. Green Areas and Community Gardens

Submissions outlined that amenities of existing properties should be protected during the construction. Stakeholders requested that any interference with community structures or spaces during the construction would be fully reinstated. Stakeholders stated that they would not support the project if it has any negative effect on the Phoenix Park, environmentally or physically. Respondents questioned whether the trees between homes and the existing Cork line are going to be destroyed. Stakeholders sought clarification on whether they were going to lose the green area and whether machinery will be left there during construction.

Stakeholders acknowledged that the project intends to take over lands with Z2 zoning designation east of St George’s Villas for attenuation facilities and raised concerns over the removal of the walled-garden and

community orchard located at this site. Stakeholders noted that this area is much loved and utilised and that the garden is used by children, adults and pets. It was highlighted in the submissions that this area was created as a community enhancement measure and an area of biodiversity with wildflowers growing in the garden and fruits that have reached “beautiful levels of growth”. Stakeholders cited that keeping biodiverse ecosystems intact is important for human health and part of the solution to climate change.

Stakeholders stated that plans to repurpose this area seem to go against the 2016 Zoning of the City plan and that the walled-in grass area’s zoning objective must be respected and the area protected. Stakeholders sought clarification on what is being proposed for the green area marked as “reserved area for the proposed track attenuation facilities”, specifically whether it will involve re-landscaping and underground or overground structures and buildings. Stakeholders further questioned what construction works would be required at this site and what impact this will have on nearby residents. Respondents sought consultation with Iarnród Éireann on the matter of the community garden and orchard “to find a solution that works for all parties”.

It was noted by residents of Seven Oaks that while they welcome the development of the southbound track, the widening of the northbound track would bring trains passing too close to the apartment complexes and encroach onto the community garden embankment. It was cited that the community garden lies right between the apartments and the existing railway lines and that it would be destroyed if the tracks are widened. It was highlighted that the community garden located at the back of the Seven Oaks is much loved by residents and attention was drawn to the importance of protecting the development’s limited outdoor space. Residents sought assurance that the complex’s boundary wall will remain as is, the boundary line of the complex will not be altered, and the tracks will not encroach on the community garden.

It was noted that if tree or planting removal is necessary, replanting nearby should be carried out as compensation. It was further noted that treatment of any marginal land or ‘leftover space’ should be considered regarding landscaping and future durability. Respondents requested that greenery either side of the tracks be preserved as much as possible to minimise the impact on wildlife. Additionally, stakeholders requested that community gardens that are temporarily repurposed during the construction phase and fully reinstated afterwards.

4.8.5. Traffic and Train Service Disruptions

Concerns about how the project will impact traffic and car travel were highlighted in the submissions, particularly in the areas of Inchicore and Kilmainham. Respondents further requested information on whether road works would be in place at the Le Fanu Bridge area. Submissions outlined that construction traffic will have to be assessed in the context of wider construction activity and the cumulative impact of several works being undertaken simultaneously. Respondents requested details of traffic levels during construction, how site traffic may affect them, traffic management systems and for a construction traffic route map to be published.

Stakeholders cited that the extensive works required to Sarsfield Bridge would result in road closures and potential difficulty accessing the Floraville apartment complex due to increased traffic volumes from the east. It was noted that it is currently necessary to do a U-turn at the junction of Sarsfield Road and Con Colbert Road to access the complex from the West and that there are frequent accidents here. It was requested that the traffic management plan for Sarsfield Bridge works factors in access to Floraville and other apartment complexes.

Stakeholders requested that disruptions to current rail services be minimised during construction. It was specifically queried whether construction will result in reduced track or platform space.

4.8.6. Rodent and Dirt and Dust Control

Submissions outlined that rodent control and management measures will be necessary during the construction to ensure the works do not create environmental health issues. Further concerns were raised about increased

levels of dirt and dust resulting from the project. The need for measures to suppress dirt and dust during loading and unloading of materials at site compounds was highlighted in the feedback.

4.8.7. Compensation / Community Gain

Residents of Kilmainham Square asked that consideration be given to the significant impact of the project on them and enquired whether compensation was being considered. Similarly, residents of ClÉ Works Estate questioned the immediate benefit there is to the estate in return for the level of disruption they will experience both in the construction phase and the day-to-day running of the new service. They queried what corporate social responsibility initiatives or commitments Iarnród Éireann will make.

Stakeholders noted that residents in the areas of Kilmainham, Inchicore and Ballyfermot will not directly benefit from these additional rail services. Therefore, they are keen to see community gain in the area. Respondents suggested that Iarnród Éireann could fund a “badly needed” new roof for the local Sports and Social Club in Inchicore as compensation for the disturbance. It was further suggested in the feedback that Iarnród Éireann would “provide opportunity for increased permeability and accessibility of the area by walking and cycling” and that this would align with the project’s goal of transport emissions reductions. Stakeholders requested that Iarnród Éireann would fund pedestrian infrastructure in the area.

Respondents questioned whether new window glazing for their property would be necessary to mitigate acoustic disturbance and it was suggested that property owners should be compensated by Iarnród Éireann for the installation of triple glazed windows or other noise blocking technologies.

4.8.8. Construction Depots

Stakeholders enquired about the location of construction sites, vehicle access points, construction compounds and the suitability of haul routes. It was highlighted that site carparking would need to be put in place to avoid overspill into unsuitable locations. Stakeholders specifically questioned whether lands in the ClÉ Works Estate will be used as for site compounds. Additionally, respondents questioned whether the Iarnród Éireann carpark would be used to locate site compounds and if so, why not the Iarnród Éireann works. The location of the site compound for works near Kylemore way was further queried.

Concern was raised that access to apartment complexes would be needed during construction as this could jeopardise security and residential parking. Respondents questioned the level of additional construction vehicles into and out of the ClÉ Works Estate via the narrow South Terrace entrance. Respondents highlighted that there is only one entrance to the estate.

Stakeholders requested that video surveys of the local road network be carried out prior to construction and that protocols be put in place for remedial works, should damage occur, and for decommissioning works, ‘to satisfactorily reinstate the sites post project’.

4.8.9. Extent of works

Stakeholders sought clarification on the extent of the works required for this project. Information on the extent of works at specific locations of the project was requested in the feedback. Stakeholders questioned whether all works for this project are within the existing rail corridor. Stakeholders requested examples of the types of structures that will be erected and an indication of their size.

4.8.10. Schedule

Stakeholders sought more information on the project’s proposed start date and duration. Information on the duration of works for specific sub-sections of the project was requested in the submissions. Local residents are concerned that they will have to live through construction for a long time. Respondents sought clarification on

what ‘Spring 2021’ means. Respondents requested that Iarnród Éireann liaise with them before actual works commence and that they were given advance notice of date and the type of construction activity occurring, particularly with night works. Respondents highlighted that at present, notice of track works is sporadic and only comes when complaints are made. This was noted as a cause for concern in the feedback.

Stakeholders queried whether construction works would take place during the day, night or both. Details of daily working hours and any weekend work or exceptional departures from normal working hours during the construction were requested in the feedback.

Respondents noted concern that the construction time will take too long to be of benefit for the climate and will increase the pollution levels in the area.

4.9. Electrification (OHLE, Substations etc.)

Submissions on electrification cited that the electrification of the train fleet should be encouraged from both environmental and noise pollution perspectives. In addition, submissions noted that electrification of the train fleet not only benefits the capacity but will also improve the overall reliability of the service.

However, concerns regarding the visual impact and property value caused by overhead power lines required for electrification were cited.

In terms of design submissions, they cited that consideration should be given to the provision of clearances for 25kV AC electrification given the likelihood this will be chosen if electrification to Cork/Limerick were to proceed in the future. In addition, queried was the power supply that would be used for the trains and the safety implications of electrification. It was queried if the existing and new lines could be upgraded and use 3kv DC and what the safety implications are of having existing overhead lines adjacent to and/or crossing the railway. Similarly, if adding overhead powerlines lead to increased exposure to electromagnetic radiation.

Noted in the submissions was the high power demand at Grange Castle Business Park and adjacent locations and the possible power supply limitation as a result of data centres for Microsoft (several sites), Google (several sites), EdgeConneX, Interxion, Equinix, CyrusOne and possibly others in the area.

Disappointment was expressed in the lack of electrification of the line to Kildare. Submissions stated that only half of the Kildare commuter line will be electrified with diesel trains still required to get to and from Naas, Newbridge and Kildare town.

With regards to substations, stakeholders requested effective consultation with them on their location in order to minimise potential impacts arising from their location. In addition, the electrical supply to the substations was queried in submissions wondering if the supply would be coming from green sources.

Stakeholders suggested that if the lines were upgraded to 3kv DC it would be more beneficial. “Acceleration”, “speed”, “less substations”, and “more regenerative braking use” were listed as some possible project improvements.

4.10. Landownership

Stakeholders note the impact the proposed works will have on their properties including the potential to permanently or temporarily damage their land. Noting that it could affect their gardens and wished for clearer, more specific plans to be published so that they are fully aware of the impact. Stakeholders questioned if the railway buildings are being moved to cater for track expansion or if garden land will be used. Submissions are concerned that the land allocated to new tracks will be closer to their residential land than previously.

Stakeholders note that often during infrastructure projects such as this that land prices are driven down. It is noted that often developers take advantage of this situation. Stakeholders wish for this to be controlled for/dealt with.

Submissions noted that although this project is to serve the wider commuter belt of Dublin, it has bypassed those who it affects most and the communities that could benefit from it.

Respondents outline that they believe the impact of the works on their land should entitle them to a method of compensation and suggest this can be achieved by providing triple glazed windows to increase their properties noise suppression abilities.

Respondents believe that it should be possible to do the works between Kylemore Road Bridge and the Khyber Pass without interfering with the private lands adjoining the line on Landen Road.

Stakeholders note that they are an active community who need active engagement from Iarnród Éireann, and should they continue to find the works an issue for the community they will appeal the proposal to An Bord Pleanála.

4.10.1. Land Acquisition & Compulsory Purchase Orders

Stakeholders are concerned with regards to how much private garden will be taken to accommodate the track expansion. They wish to be notified of the impact temporary or permanent land take might take place.

Stakeholders wish it would be clarified if the Iarnród Éireann land will be used first before any private land is taken for the tracks. Clarification on the land along the Landen road being taken for the project was asked by stakeholders.

Stakeholders noted that due to the lack of stations, that the noise pollution and land acquisition near their property is not worth the impact it will have.

Stakeholders believe that if any land/garden is taken for the project that they should be adequately compensated as a result. Stakeholders who have shed/structures in their gardens wish for them to be replaced at the cost to Iarnród Éireann, should they be impacted in the process of land acquisition.

Stakeholders are concerned with the possible compulsory purchase orders that may take place. They wish for the technical drawings to fully outline what land is being taken that they be informed of any CPO to gardens or residential land. Stakeholders state that although they were informed no CPO would take place. They note that one CPO was issued and involves a listed wall and wish for this to be kept.

Submissions note concerns for those that are currently selling property along the route, stating that the threat of CPO will prevent any possible sale and wish for letters to be issued confirming if any CPO will take place.

Stakeholders state that CPO's are an infringement on people's rights and should not take place.

Stakeholders note that any land acquisition or compulsory purchase orders must appropriately compensate those effected and strictly follow any legislation pertaining to the process.

They note that any works to restore land back to a proper condition should take place after any CPO, to minimise the impact on the owner.

Stakeholders note that should any land acquisition or CPO take place that the air rights are retained by the owner.

Stakeholders note that agreements should be made to appropriately redress the situation faced by landowners, including compensation, and remedial/landscaping works.

Submissions stress that land agreements should be made prior to the commencement of works to ensure stakeholders are properly informed and redressed.

4.10.2. Property

Stakeholders listed concerns for their properties. They note that they were not engaged fully with for these works, that it will have no benefit for them but will increase the noise, traffic and possible anti-social behaviour in their areas. Stakeholders note that without access to the line that no benefit for them is seen.

Submissions received stated that they wish for clarity on the works, particularly in residential areas where private property maybe affected or taken to accommodate the track expansion. They state that although construction on the tracks will end, track maintenance and an increase in trains passing will make their properties unliveable. Stakeholders issue their concerns that they may have to invest money into sound proofing their properties and believe this cost should fall on Iarnród Éireann.

Stakeholders note that green space taken should be restored and trees replanted to aid blocking the view and noise absorption. However, leaf-slippage is noted as an issue for consideration when planting the trees

Stakeholders queried the access that the public will have once the proposed works have been completed. They are concerned that the public will have access to their gardens.

Submissions noted that the works are likely to have a significant impact on the area and wish for access to their residential properties to be considered and ensure that access is always provided. They note that daily traffic will increase and with only one entrance to come residential areas it may be hard to access them.

Stakeholders expressed their concerns to the possible damage that may occur to their properties as a result of the proposed works and the increase in trains on the line as a result.

Structural integrity of buildings was raised by stakeholders as a key issue they wished to be addressed. They note that often the buildings shake slightly as trains pass and that vibrations can be felt when trains idle on the tracks. They state that the tracks are currently alongside the foundations to their building and worry that the increase in vibrations and construction works may affect the buildings. They wish for appropriate studies and inspections to take place to confirm no lasting damage will be done. Submissions state that the instalment of anti-vibration plates or other vibration mitigation measures should be implemented.

Stakeholders state that unless assurances are given, to the satisfactory of property owners, as to the possible impacts the proposed works may have on their properties, legal challenges will be sought.

Stakeholders expressed that if any impact to properties was to take place that they should be informed as to the extent.

Stakeholders are concerned that the proposed works will have a resulting impact on their property values.

Stakeholders note that this project only damages the area and its value and does not benefit the community it affects. Noting that the plan will cause considerable disruption and reduction in the value of properties with no return. They state that the scale of the current track maintenance and the proposed works will render residential properties unliveable and due to the works, unsellable or rentable. Stakeholders note that the addition of overhead electricity lines will impede the view from residential buildings further restricting the view and the sale value as potential buyers will not want to view the lines.

The noise from the proposed works, the increase in train traffic noise as a result, the possible air pollution and the cutting down of trees are noted by submissions as decreasing factors for property values. The removal of green spaces and the possibility of a high wall being erected between resident buildings and the tracks also concerns stakeholders.

4.11. Bridges

Feedback suggested that any alterations to existing bridges across the rail line be “future-proofed” and a common theme for all bridges was the addition of capacity to accommodate further expansion for cycle and pedestrian paths. Stakeholders also raised concerns that any land take required for bridge alterations be minimised so as not to affect private lands.

4.11.1. Khyber Pass Footbridge

Stakeholders expressed concerns about the lack of public access to the Khyber Pass footbridge and inquired if it could be made public as this footbridge is currently only accessible by Iarnród Éireann workers. Some stakeholders felt that if made public, the bridge could give access to the LUAS red line and other services and would benefit the area.

However, further submissions suggested that access to the Khyber Pass footbridge should remain private and not become publicly accessed and safety concerns were raised with regards to possible anti-social behaviour that may develop as a result of works to this bridge, particularly if it becomes open to the public. Submissions were received requesting increased security measures to ensure safety in the vicinity of the footbridge.

Stakeholders expressed concerns about the impacts to this footbridge during the project. They believe that the bridge may require extensive works and further clarity was sought on the plans for the Khyber Pass footbridge. It was also noted that the bridge should have accessibility for wheelchairs and bicycles and the provision of a spiral stairs, or a lift was suggested. Submissions were received with regards to the Khyber Pass footbridge restricting resident’s views, which they believe could be worsened by any works the bridge may require in the proposed plans.

4.11.2. Kylemore Road Bridge

Stakeholders raised the possibility that this bridge could become an interchange point to Bus Connects from the new proposed rail line. Respondents raised concerns about the current condition of the bridge for the proposed developments. They expressed concern regarding the safety of the bridge including if the vertical clearance was adequate for the expansion to four tracks. Stakeholders stated that the bridges parapets appear to be in poor condition currently.

4.11.3. Le Fanu Bridge

Stakeholders highlighted the lack of cycling and pedestrian access to this bridge and wish for improvements to be made in relation to these. Stakeholders note that bridge works will be needed to improve the clearance of the bridge and suggest that a combination of raising the bridge and track lowering. Stakeholders also noted that safety improvements, in terms of pavement and public safety are needed. Respondents want to ensure that a comprehensive Environmental Impact Assessment Report is done in this area to ensure the work does not have any unforeseen effects/impacts.

Stakeholders stated their concerns with regards to the technical drawings and the boundaries drawn on them. The stakeholders believe from these drawings there will be encroachment onto private land. They wish to get confirmation about the impact, if any, onto private land. Stakeholders furthered this by saying they did not want any bridge works to encroach onto green spaces as this is where children play.

Further to these submissions, safety was also noted as an issue for stakeholders. They wanted to ensure that the Le Fanu Bridge was safely developed for both the rail line and pedestrian/bicycle/wheelchair access. Stakeholders wished to have public access improved prior to the commencement of the main works. Submissions noted that the Le Fanu bridge requires widening for this to be achieved. Respondents noted that emergency access should be designed into the works on the bridge.

Stakeholders cited concerns that the works to Le Fanu Bridge and the access to the bridge will become a centre of anti-social behaviour and that it will provide a means of access/escape for those who wish to commit illegal activity. Stakeholders suggested that the designers/engineers reconsider the impact the developments of the bridge will have on the area and not just focus on the bridge itself.

4.11.4. Liffey Bridge

Stakeholders noted that any improvements the Liffey Bridge needs to include improved access for bicycles and pedestrians as it provides a vital link to other areas.

4.11.5. Memorial Road Bridge

Stakeholders enquired as to the impact that works to Memorial Bridge will have on private lands. Submissions noted the need for the bridge to be expanded to cater for a two-way cycle track to align with future cycle path developments in the area. The submissions state that improvements to this bridge will improve the access for those using sustainable and public transport.

4.11.6. Sarsfield Road Bridge

Submissions were received that stated the underpass at Sarsfield Road is very narrow but preferred the option to do minimum works. However, submissions did note that works were required to widen the 'inappropriately narrow' underpass.

Stakeholders note that the rail crossing over Sarsfield road should be extended so that the road can be widened to fit cycle and pedestrian needs, noting that it is currently a 'pinch-point'.

Respondents detailed that the extensive works proposed for the Sarsfield Road Bridge would entail disruption to traffic in the area, which is currently at a high volume. The submissions state that an improved traffic management system is needed in the area and for access to residential areas.

Submissions note that a station at Sarsfield Road Bridge building would serve a large catchment area and that if the station is not built in these works that space should be left for future developments.

Stakeholders note that the Sarsfield Road bridge require improved access for pedestrians and cyclists who wish to use the improved rail lines.

Submissions noted that the current plans do not take note of the current configuration of the Sarsfield Road underpass. Noting that it is currently a 'bottleneck' and prevents the free flow of pedestrian and vehicular traffic. It is suggested that the level of the tracks is adjusted and levelled between the Sarsfield Bridge and the Khyber Pass bridge to improve the traffic flow and create space for a station.

Stakeholders note that often after periods of rain that excess water pools under the bridge and this needs to be addressed.

4.11.7. South Circular Road Bridge

Stakeholders note that as significant works will be taking place at the bridge, that the opportunity should be taken to redesign the current junction to make it more accessible to pedestrians and cyclist which could improve the access to the rail line from other nearby areas.

4.12. Safety

Safety of children was highlighted in the feedback with stakeholders outlining that children play on the roads and will be at risk of injury during construction. Stakeholders highlighted that safety of pedestrians and cyclists should be considered when completing works in raising road levels.

Further concerns for the safety of the public involved design around track security, namely guarding against trespassing and preventing accidental electrocutions.

Safety concerns were raised regarding access to the complex to carry out the works which could impact on parking within the complex and increase security risks.

Stakeholders noted concerns of 'reserved areas of or attenuation facilities' near residences and the safety impacts this may have.

Stakeholders raised concerns of capacity issues and overcrowding on all trains during rush hours which could lead to safety issues for users. Feedback noted more trains were needed in order to reduce overcrowding that is currently seen on the service.

COVID-19 and the health risks of passengers was highlighted in submissions, with concerns over capacity and ensuring users can travel using social distancing.

In terms of risk assessment, feedback stated that a risk assessment needs to be carried out on the Phoenix Park Tunnel considering that this project will increase train capacity through it, and that this need is required as there will be commercial rail services going through with chemicals.

Furthermore, it was stated that a previous risk assessment was undertaken on the tunnel by Michael Slattery & Associates however this was 'only for eight passenger movements per day.' It was further noted that reported access to the north of the tunnel has regressed and that steps from the Garda Pound need to be added to ensure access into the tunnel.

Stakeholders asked that security measures be placed around the electricity sources to guard against trespassing and or accidental electrocutions by members of the public.

Submissions queried the safety of road raising for pedestrians and cyclists and wished for clarification to be relayed back. Bridge height for busses was regarded as a priority to ensure the impact of the works does not affect commuters using other public transport options.

4.12.1. Safety of Design

Submissions offered suggestions of design of stations. This included level/step free access provided on both sides to maximise accessibility and minimise walking distance to the station; providing ramps; ensuring each access point is provided with ticket selling and validation equipment and passenger information; and providing multiple exits if possible to maximise local access. Further station design suggestions included provision of a well-lit, covered, and secure bike parking at all stations; provision for bus stops with appropriate parking bays and shelters; and open plan stations without turnstile barriers to reduce station size and improve passenger flows.

Safety in design of bridges was cited by stakeholders, particularly in allowing the opportunity to expand the bridge width, allowing more safe space for pedestrians and cyclists to cross the bridge.

Suggestions of extending the rail crossing over Sarsfield Rd was highlighted in the submissions, so that the road can be widened to fit cycle and pedestrian needs.

Stakeholders queried if Khyber Pass will open for public access and if increased security measures will be put in place while works are ongoing, to prevent any anti-social behaviour in the walkway.

Further safety design was questioned in the submissions including if there will be sufficient clearance underneath bridges for buses, ideally 4.65- 5.00 metres, noting legal maximum height and normal construction height for bridges, subject to safety margins and dynamic vehicle envelopes. Suggestions that part of this could be achieved by lowering the road were cited.

Stakeholders highlighted that currently the junction design at the bridges does not meet the minimum standards on a variety of metrics according to the Design Manual for Urban Roads and Streets. It was stated that considering the volume of work required on these bridges for DART+, local residents need assurances that the safety and comfort of the pedestrian user is dramatically improved with traffic calming and pedestrian priority design, and that these improvements should be made in co-operation with the BusConnects plans for the area.

Feedback suggested that pedestrian crossings should be raised, footpaths should be at least 1.8m wide and corner radius at junctions should be reduced to prevent pedestrians being injured by vehicles.

4.12.2. Safety of Trains in Tunnels

The Phoenix Park Tunnel was highlighted by stakeholders as a safety concern. The Tunnel was noted to produce a series of aerodynamics problems, such as aerodynamic drag force and pressure wave, which causes a significant increase in train energy consumption, shortens the life of train/tunnel, increases maintenance cost, increases passenger discomfort and limits the speed of train in the future. It was suggested that the project team consider a wider tunnel instead of two narrow tunnels if possible.

4.12.3. Traffic Management Safety

Traffic management safety concerns were highlighted around Sarsfield Road and Con Colbert Road junction, where 'frequent traffic accidents due to speeding' were cited. For this reason, stakeholders highlighted that traffic management plans for the Sarsfield Road Bridge Works must be in place, including for Floraville and other complexes in the area.

Stakeholders advised that the junction at St. John Roads West should be completely re-designed to make a better entrance to the city and to encourage modal shift away from driving.

Submissions noted that the project will significantly reduce the road traffic using the Con Colbert Road and therefore providing traffic calming and safe pedestrian friendly crossings is needed.

Another area of concern cited by feedback was the junction at South Circular Road and Chapelizod Bypass which is considered to be high-traffic and 'hugely hostile' to pedestrians and cyclists. Feedback urged the project to use this opportunity to make substantial changes to this junction to allow active travel priority and improve safety.

Stakeholders queried the works pre and post construction and how this will impact the existing Inchicore works estate, in terms of construction travel levels.

4.13. Operational Phase / Post Construction

4.13.1. Impact on Local Residents

Respondents residing near the train track are concerned about the increased volume of trains in the operational phase. More information on the day-to-day impact of the new service for local residents was requested in the submissions. Stakeholders further requested clear and transparent communication from Iarnród Éireann on the matter. Stakeholders noted that they are continually disrupted by trains passing and stated that it is a downside to living in the area. There is concern amongst stakeholders that the disruption caused by passing trains will be augmented with the increased frequency of trains. Property owners are concerned that the value of their

properties will decrease as a result of the increased disturbance associated with the new service. Respondents questioned whether longer carriages could be used as an alternative to increased frequency.

Stakeholders are particularly concerned about noise levels and acoustic disturbances. It was noted in the submissions that the area around Heuston to Memorial Bridge is mapped by the Environmental Protection Agency as having one of the highest levels of day and night time noise in the city and that this project will only increase this. Respondents welcomed that electrified trains would reduce noise pollution.

Stakeholders urged Iarnród Éireann to consider noise reduction strategies and technologies, such as track silencing, soundproofing nearby properties and erecting sound barriers. Stakeholders noted that the increased volume of trains will result in increased sounding of train horns. It was noted that this is particularly disruptive and at present occurs in the early morning and late at night. Stakeholders questioned whether Iarnród Éireann could consider alternative warning systems, specifically at the Phoenix Park tunnel.

Stakeholders are further concerned about increased vibrations and tremors resulting from the new service and similarly urged Iarnród Éireann to consider mitigating measures. Local residents questioned whether the increased volume of trains and therefore increased vibrations and tremors will have a structural or foundational impact on their properties.

Stakeholders raised concerns about the project's impact on air quality in the area.

4.13.2. DART+ South West Service

Stakeholders welcomed the new service, acknowledged that it is necessary and noted that they are interested in seeing improved public transport in the city. The increased capacity was welcomed by stakeholders, who noted the current issues with overcrowding. Stakeholders cited that ensuring that there is sufficient frequency of trains will make public transport a more attractive option and reduce car travel. Stakeholders noted that trains are more comfortable and reliable in comparison to buses. Respondents highlighted that they want the trains to run on time, like the current Greystones to Connolly DART service.

Stakeholders highlighted that they welcomed the electrification of the line. Stakeholders questioned whether all four tracks could be electrified and noted that this would result in improved operational flexibility of the line. Respondents queried whether upgrading existing and new lines to 3kV DC would be more beneficial and cited studies from Spain and the Netherlands. Stakeholders further enquired whether vertical clearance on the route would facilitate double deck trains in the future.

Stakeholders cited that they would like to see increased late night services and potentially a 24-hour service to support Dublin's night-time economy. Similarly, stakeholders requested that there would be a good weekend service. Stakeholders questioned whether InterCity and existing commuter trains would be able to use the DART+ tracks and whether these trains would stop at stations along the DART+ route.

Submissions outlined that this project would open access to commerce, leisure and tourism, encourage the development of local communities, reduce the time commuting to the city centre and provide local residents with greater access to shops and services. Additionally, it was noted that increased footfall will result in increased customers and revenue for businesses near stations.

Stakeholders noted that the increased volume of passengers will require upgraded pedestrian access to stations to ensure stations can be accessed comfortably and safely. Similarly, it was noted in the feedback that bicycle parking facilities should be improved in stations. Stakeholders suggested that bicycle storage on board the DART+ South West trains could be considered, to facilitate multi modal transport. Stakeholders requested that the price of parking at stations be reduced and that security at stations should be improved. It was noted in the feedback that stations should have serviced public toilets. Stakeholders further sought assurance that there would be more room in the carriages.

4.13.3. Integration with Other Public Transport Services

Stakeholders were pleased to see that the project intends to integrate with other transport services, particularly the Glasnevin Metro. It was noted in the submissions that this project will bring greater connectivity and choice. Respondents cited that they would like to see seamless integration with other public transport services. Stakeholders noted that this project will enable them to use the DART to access InterCity services at Heuston. Additionally, stakeholders noted that access to the North City via the Phoenix Park tunnel opens greater opportunities for commuters. Stakeholders queried whether there would be increased trains from Park West to the Docklands. Stakeholders further sought clarity on what was being proposed in relation to trains running through the Docklands. Concern was raised that this project will affect existing InterCity and Commuter services.

It was noted in the submissions that this project will increase demand for LUAS services and therefore capacity on the services should be increased as they currently experience overcrowding. Stakeholders highlighted that there should be an underground service from Heuston to the city centre and that having to loop via Glasnevin or Drumcondra would not entice people to use this service.

4.13.4. Stations

Concern was raised that there are not enough stations along the route and that some communities in urban areas that will be disrupted by this project are being deprived of access to modern transport. Submissions outlined that the DART would pass by densely populated areas with ample demand and this counters the National Strategic Outcome of 'Compact Growth'. Stakeholders noted that they understand why the line becomes an 'express' between Park West and Glasnevin Metro Station but feel that areas between need to be served. It was suggested that an intermittent 'local' train from PW to Glasnevin, serving Inchicore, Heuston and Cabra could be implemented. Feedback questioned why the construction of new stations was not being integrated with bridge works in this project and it was noted that this would reduce cost and disruptions.

4.13.5. Miscellaneous

Stakeholders suggested that overnight stabling for trains should be included immediately left of Hazelhatch station, otherwise they would have to travel back to Heuston.

Feedback suggested that 'Heuston West' station should be renamed 'Island Bridge' as it would be more inclusive of the community in which it resides and would contribute to local identity

4.14. Four tracking

Stakeholders note the substantial works required to the current rail line to expand it to the proposed four tracks but are concerned about how much residential land will be removed as a result and if it will be bought by compulsory purchase orders or if the non-residential side of the tracks will be sufficient space for the expansion.

Stakeholder feedback recognises the need for four tracking and subsequent electrification to increase the services provided, not just now but in years to come. They do question why only two tracks are being electrified and that it would make more sense to convert all four tracks upon completion to electric. Stakeholders also question if the use of welded tracks has been considered as part of this project.

The electrification is noted by submissions as being a positive feature of the four tracking, but they raise the issue that it may have an impact on current services and why it can't be rolled out across all the DART lines, including rail lines to other cities. They note the benefits of four tracking to be; the ability to provide more services, little interference between train lines, increased capacity and faster journeys for commuters.

Feedback suggested that the project might be better be made underground. They do note that this will be a more labour intensive and expensive approach, but they claim that none of the proposed track modifications between Hazelhatch and Park West would be required if the DART underground preceded this project.

Stakeholders are concerned about the noise increase from two to four tracks.

Submissions note that in some areas of the proposed works there are currently three tracks and question if there will be one or two additional tracks added.

Submissions state that a bottle neck of trains exists near Cherry Orchard as the lines are shared with the intercity services and note that this issue needs to be resolved. They note that four tracking all the way to Heuston is a key to enhancing the line.

Submissions state that the proposals must include two track approaches with 4 electrified platforms and conversion of the siding next to the platform, is work that is required to ensure optimal usage across services and platforms.

Respondents noted that recent four tracking and reconstruction of several bridges has occurred between Clondalkin/Fonthill stations and Kishoge stations, so only minimal works was required here.

Stakeholders did note however that this project should not be extended further west from Hazelhatch and Celbridge until there are also plans to quadruple the track line west.

The Khyber Pass footbridge and the possible works it needs for the four tracking concerns stakeholders. They state that the bridge currently restricts the view from residential complexes in the area and fear this will be worsened when it is increase in size to accommodate the track works.

4.15. Surveys & Site Investigations

Feedback stated sufficient archaeological, heritage and architectural studies and investigations need to be undertaken in areas where these features and structures exist.

Regarding architectural heritage, submissions noted that almost all of the highlighted Architectural Heritage lies within the Kilmainham and Inchicore area and that it is imperative that due recognition and preservation of these sites is upheld while works are ongoing.

Submissions further noted that there are 'a series of existing overbridges along the proposed line that need to be structurally assessed within this project.'

A stakeholder offered the project team use of a drone to capture high resolution photos of the tracks around Cabra Station to give 'a clear timely view of whether development work is currently encroaching on the space necessary for a future station.'

5. Next Steps

5.1. Summary

All submissions received as part of this first round of public consultation will feed into the option selection process and the selection of the Preferred Option. The project team have analysed the submissions and considered all relevant information in the evaluation of the Multi Criteria Analysis (MCA) for the next stage of the project. This evaluation also includes the further refinement of the options design and additional elements which will be brought forward in the selection of the Preferred Option.

The key issues arising from public and stakeholder feedback from PC1 are dealt with under the following headings:

- Matters outside of the Scope of the Project.
- Matters which require Further Assessment for the Preferred Option.
- Matters to be Addressed by Ongoing Design Development and the RO Application and EIAR.

5.1.1. Matters Outside of the Scope of The Project

New Railway Stations

A significant number of submissions during PC1 called for new railway stations along the railway line, including at Kylemore, Cabra and Heuston West.

The scope of the DART+ South West Project considers the necessary railway infrastructure to enable increased rail capacity and transition to electrical power. While the provision of new stations does not form part of this scope, consideration has been given to potential future stations during design development, including track alignments and other infrastructure which would not preclude the delivery of new stations in the future.

The National Transport Authority published the draft Transport Strategy for the Greater Dublin Area 2022-2042 in November 2021. A number of new stations have been identified in the draft Strategy, including at Kylemore, Cabra and Heuston West. Following the electrification and upgrade of the commuter lines, NTA has committed to developing these stations in conjunction with Iarnród Éireann to provide higher levels of public transport accessibility at locations which currently accrue little gains from the presence of a rail corridor.

In the case of Heuston West, the scope of the DART+ South West Project included a Feasibility Report and Concept Design for a potential new station at this location. Having regard to public feedback, the progress made on the Feasibility Report and Concept Design, and having regard to the location of the potential station within Iarnród Éireann lands at Heuston (and more specifically at the location of the existing platform 10), Iarnród Éireann has made the decision to include the new Heuston West Station in the scope of the Project to be brought forward for Railway Order (RO). The inclusion of an intermediate station between Park West & Cherry Orchard Station and Glasnevin Station at Heuston will address the concerns and opportunities identified by the public relating to servicing the local community and multi-modal interconnectivity.

Extending the DART+ South West Project

Some submissions requested the extension of the Project to Sallins / Naas.

The National Transport Authority published the draft Transport Strategy for the Greater Dublin Area 2022-2042 in November 2021. The draft Strategy identifies that forecast demand for travel, when considered in tandem with the need to reduce transport emissions, has shown that, over the lifetime of the Transport Strategy, there will be a requirement to further extend DART services to key locations in the GDA. An extension of the DART service

on the Kildare Line to Naas / Sallins will provide additional capacity to this area, including to a planned regional Park & Ride site in this vicinity.

Facilities at Existing Railway Stations

A significant number of submissions raised concerns regarding existing facilities at stations. All concerns have been noted and passed to the relevant teams within the DART+ Programme who will assess each issue in greater detail, including

- **Pedestrian and Cycling Facilities:** Pedestrian and cycling facilities associated with many of the existing stations were provided as part of the original Kildare Route Project; the facilities are constantly under review and are the remit of the Iarnród Éireann Station Enhancement Programme.
- The provision of strategic Park & Ride facilities and car parking at or near existing train stations is not part of the DART+ Programme. However, the NTA's Park and Ride Development Office is currently working with Iarnród Éireann to identify strategic locations to develop park and ride schemes that will connect with the rail system. Proposals will be brought forward independently of the DART+ Programme.

Matters which Require Further Assessment for the Preferred Option

The purpose of PC1 was to present the Emerging Preferred Option for the proposed DART+ South West Project and to request the views of the public and stakeholders. All submissions received as part of the first round of consultations have fed into the design process, an updated option selection process and the identification of the Preferred Option.

The Project Team has analysed the submissions and considered all relevant information of potential relevance for the re-evaluation of the optioneering to date. As part of this analysis the following items or options were identified as requiring further consideration and have been considered in the options re-evaluation process:

- The inclusion of the new Heuston West Station in the scope of the Project to be brought forward for Railway Order (RO).

As this station is located wholly in Iarnród Éireann's Heuston Station boundary and having regard to the requirements for the station the options for assessment are not materially different and are therefore largely a technical matter (relating to design and access) which was subject to MCA.

- Following feedback and more detailed design of the four tracking requirements between Kylemore Bridge and Khyber Pass Footbridge, it is possible to avoid removing a turret associated with a locomotive shed to the south of the line. This structure is listed in the National Inventory of Architectural Heritage (NIAH) (Reg. No. 50080418) located within the Railway Works at Inchicore.

In the MCA which identified Option 4 as the Emerging Preferred Option and was determined to have 'Some Comparative Advantage' over Option 3 in respect of the CAF criteria of Economy, Integration and Environment. However, due to the removal of both the Signal Box and Turret in Option 4, Option 3 (which only involved removal of the Signal Box) was found to have 'Some Comparative Advantage' in respect of the specific Cultural Heritage and Architectural Heritage and Biodiversity (potential for bat roosts) sub criterion; however, this did not change the overall assessment findings for Environment favouring Option 4.

The Stage 2: MCA was re-run in respect of the options for Inchicore Works to account for the fact that following more detailed design it is possible to avoid impacting the Turret. Both options are now found to be 'Comparable to the other option / neutral' in respect of both the Cultural Heritage and Architectural Heritage and Biodiversity criterions. This did not change the overall assessment findings of Option 4 as the Preferred Option and it was subject to more detailed design leading to the identification of the Preferred Option which is presented in this report.

With the exception of the above elements, neither the additionally sourced baseline information nor outcomes of design development since PC1, inclusive of stakeholder input, have materially impacted the optioneering and the MCA findings presented in the POSR (and the Emerging Preferred Options for four tracking and bridge replacements).

However, cognisant of the level of feedback relating to construction and operational environmental impacts we have also sought to provide additional information relating to the construction technologies and methodology so that the public may understand the approach being considered. It is acknowledged that this information is based on information and level of design available at this time and it will continue to be developed as part of the Railway Order package and supporting documentation.

5.1.2. Matters to be Addressed as part of Ongoing Design Development, the RO Application and EIAR

All feedback relating to environmental matters has been fed back to the Project Team, including environmental specialists inputting into the Environmental Impact Assessment Report (EIAR) which will be submitted with the Railway Order. This includes inter alia:

- Traffic & Transportation – the potential impact of temporary bridge closures on the surrounding community, the potential impact of works for pedestrians and cyclists; and appropriate mitigation (traffic management measures).
- Air Quality – the potential benefit arising from the introduction of electric trains and potential impact at a local level in terms of dust and air pollution affecting nearby residents.
- Archaeology and Cultural Heritage - potential impact on heritage and cultural sites including the area surrounding Glasnevin.
- Architectural Heritage – potential impact on various buildings and bridges of significant architectural heritage importance including those in Iarnród Éireann Inchicore Works Estate (including the Signal Box and Turret and around Memorial Park; also appropriate mitigation required in respect of any buildings of architectural heritage which must be impacted by the works. Consideration of the social heritage impact arising was also flagged.
- Biodiversity – potential impact on local biodiversity (including uncultivated areas along railway cuttings and embankment); potential for protected flora and fauna including badgers, bats and otters; also appropriate mitigation in terms of reinstatement and replacing / enhanced planting where tree removal is necessary. Some stakeholders expressed concerns about the proposed line and its possible impact on the Phoenix Park
- Climate – the potential impact and contribution the electrification of the line will have in assisting the achievement of EU greenhouse gas emission targets and facilitating modal shift away from the private car.
- Human Health – the potential noise impact of both daytime and night-time works along the track and operational noise resulting from the increase in the frequency of passing trains and the potential impact of exposure to electromagnetic radiation.
- Land and Soils – the potential impact of the proposed works on the stability of embankments affecting roads or buildings surrounding them.
- Landscape and Visual - the potential impact of the proposed works on buildings and bridges of architectural heritage importance and the loss of green spaces and planting; also appropriate mitigation

in terms of reinstatement and replacing / enhanced planting where tree removal is necessary and careful design of replacement bridges and the siting of portal structures.

- Noise and Vibration - the potential noise and vibration impact of both daytime and night-time construction works along the track and operational noise resulting from the increase in the frequency of passing trains.
- Population – the potential impact on communities, including on community facilities (e.g. a community orchard, and communal gardens)

Key feedback came from potential impacted residents and local businesses and related to concerns in respect of the extent of temporary or permanent land take required. Some requested clarification that the Iarnród Éireann land will be used first before any private land is taken for the tracks; others noted that agreements should be made to appropriately redress the situation faced by landowners, including compensation, and remedial/landscaping works.

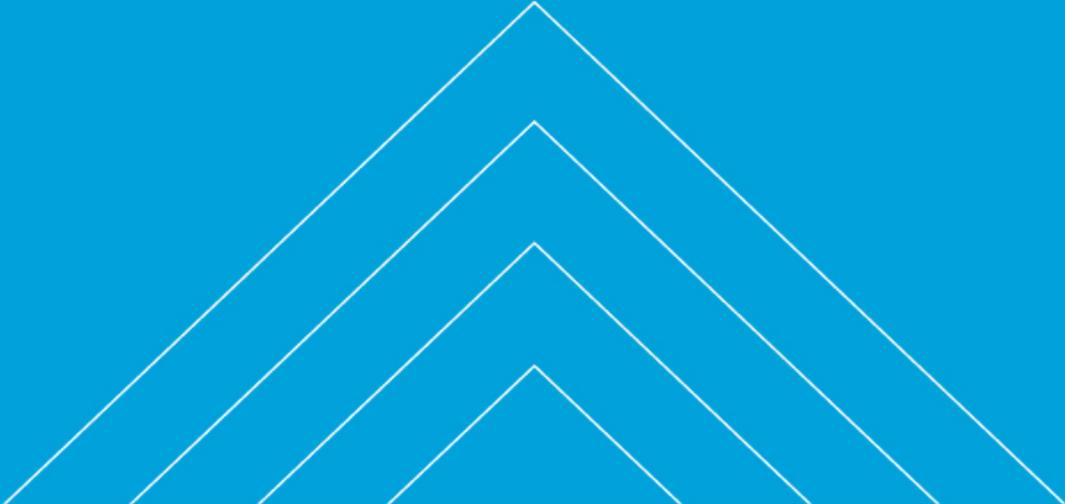
The key starting principle for the Project, is to upgrade the existing railway and to undertake all works within the existing railway corridor. This can be achieved over the majority of the route. However, public and private land will be impacted by the Project, and the acquisition of land and/or property and other interests (including new rights), whether whole or in part, will be necessary. However, detailed design and technical and construction related solutions will continue to seek to minimise this up to the submission of the Railway Order.

The Option Selection Report provides an update on the potential impact of the Project outside of land owned by CIÉ.

5.2. Next Steps

Following the update of the options assessments the Preferred Option will be presented at Public Consultation No. 2 which is planned for Winter 2021. All feedback received on the Preferred Option at Public Consultation No. 2 will feed into the development of the preliminary design, Railway Order and Environmental Impact Assessment Report (EIAR).

Appendices



Appendix A. Sample Media Coverage

Media Name: TheJournal.ie

Date: 12th May 2021

Available online at: <https://www.thejournal.ie/DART-public-consultation-5435271-May2021/>

thejournal.ie Contribute: [Support us now](#)

Irish News FactCheck Voices The Good Information Project Covid-19

Public consultation launched on extension of Dart line from Heuston to Hazelhatch

Once this consultation is completed, all submissions will be considered by the project design team.

May 12th 2021, 1:40 PM 17,626 Views 32 Comments Share 6 Tweet Email

THE FIRST ROUND of public consultation has begun to extend the Dart line from Heuston to Hazelhatch.

It is planned that the Dart+ South West project will deliver an improved electrified network, with increased passenger capacity and enhanced train performance between Hazelhatch and Celbridge and Heuston, and the Glasnevin/Docklands area via the Phoenix Park Tunnel.



Image: Sam Boal

The project is expected to:

- Continue the four-tracking of the rail line from Park West & Cherry Orchard Station into Heuston, extending the works completed on the route in 2010.
- See track improvements and bridge modifications, where necessary, to facilitate electrified train services.
- See the Phoenix Park Tunnel upgraded to allow for significant extra services directly into the city centre.
- Include a feasibility study and concept design will be undertaken for a future new station at Heuston West.

The project is being funded by the National Transport Authority and the Innovation and

Media Name: Irish Building Magazine

Date: 14th May 2021

Available online at: <https://www.irishbuildingmagazine.ie/2021/05/14/public-consultation-begins-for-DART-south-west/>



Public consultation begins for DART+ South West

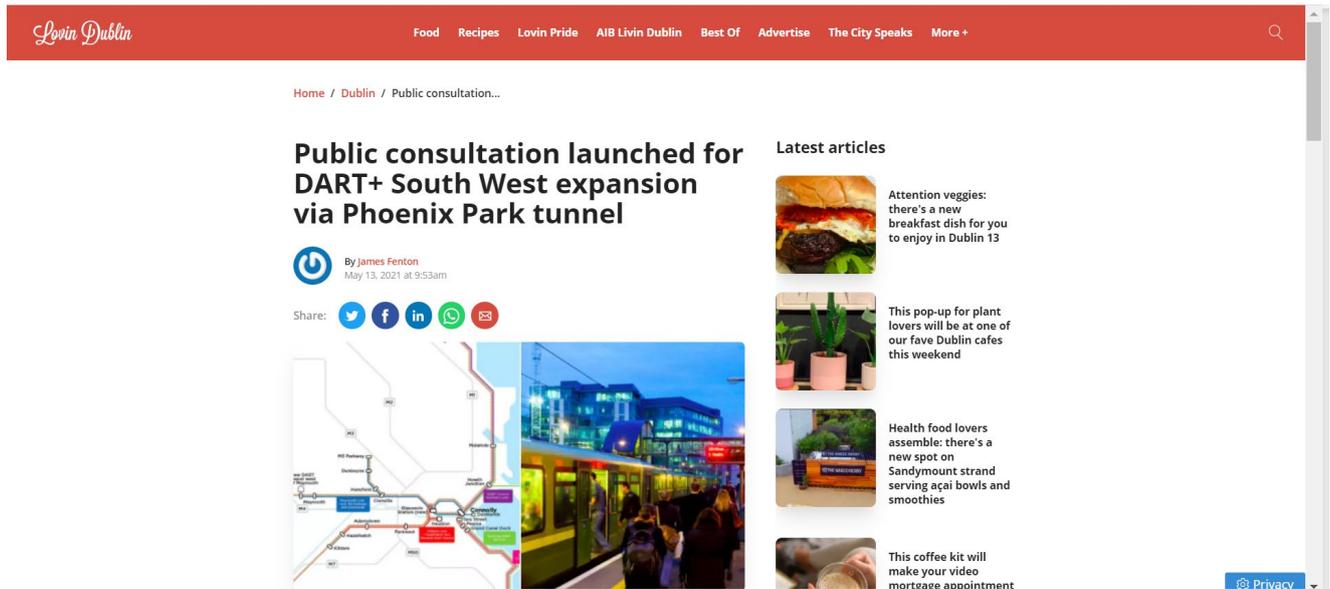
May 14, 2021 788 Views

Minister for Transport Eamon Ryan T.D.,
Mayor of South Dublin Ed O'Brien,
Cathairleach of Kildare Co. Co. Mark Stafford

Media Name: Lovin Dublin

Date: 13th May 2021

Available online at: <https://lovindublin.com/dublin/public-consultation-launched-for-DART-south-west-expansion-via-phoenix-park-tunnel>



The screenshot shows the Lovin Dublin website interface. At the top is a red navigation bar with the site logo and menu items: Food, Recipes, Lovin Pride, AIB Livin Dublin, Best Of, Advertise, The City Speaks, and More+. Below the navigation bar is a breadcrumb trail: Home / Dublin / Public consultation... The main article title is "Public consultation launched for DART+ South West expansion via Phoenix Park tunnel" by James Fenton, dated May 13, 2021 at 9:53am. The article features a map of the DART+ South West expansion route and a photograph of a DART train at a station. To the right of the article is a "Latest articles" section with four featured posts: "Attention veggies: there's a new breakfast dish for you to enjoy in Dublin 13", "This pop-up for plant lovers will be at one of our fave Dublin cafes this weekend", "Health food lovers assemble: there's a new spot on Sandymount strand serving açai bowls and smoothies", and "This coffee kit will make your video mortgage appointment". A "Privacy" link is visible in the bottom right corner of the page.

Media Name: News Group

Date: 14th May 2021

Available online at: <https://www.newsgroup.ie/DART-south-west-public-consultation-begins-to-extend-DART/>



GROUP NEWSPAPERS info@kkwindows.ie Monday – Friday 9am - 5.30pm
www.kkwindows.ie & Saturday 10am – 2pm K&K Windows LTD

Home Dublin News ▾ Motoring Education Employment Property Business Competitions Book Online 🔍

DART+ South West: public consultation begins to extend DART

👤 Sarah Brooks • 📅 May 14, 2021 • 📁 CATEGORY - Clondalkin News, Community News, South Dublin County Council, Transport



Minister for Transport Eamon Ryan T.D., Lord Mayor of Dublin, Hazel Chu, Mayor of South Dublin, Ed O'Brien and Cathaoirleach of Kildare Co.Co. Mark Stafford joined Iarnród Éireann's CEO, Jim Meade formally opened the first round of public consultation on the Emerging Preferred Option on DART+ South West at Heuston Station recently. Funded by the National Transport Authority, under Project Ireland 2040 and the Innovation and Networks Executive Agency (INEA) with powers delegated by the European Union TEN-T core network, the DART+ South West project, as part of the DART+ Programme. The project will deliver an improved electrified network with increased




Media Name: Dublin Live

Date: 12th May 2021

Available online at: <https://www.dublinlive.ie/news/dublin-news/cabra-left-out-DART-expansion-20581166>

News > Dublin News > Cabra

Cabra left out of DART expansion 'wrong decision' with NTA slammed and told to 'cop on'

A local councillor said the area's public transport is already over capacity

SHARE     COMMENTS By [Laura Lyne](#) Reporter 17:43, 12 MAY 2021 NEWS



Privacy

RECOMMENDED

 Amy Huberman escapes from Dublin as she enjoys weekend away with the girls 

 Gardai pounce arresting boy after

Media Name: Independent.ie

Date: 14th May 2021

Available online at: <https://www.independent.ie/irish-news/what-good-is-an-expanded-DART-system-if-dubliners-cant-use-it-plan-criticised-over-lack-of-new-stations-40424901.html>

Independent.ie

News Opinion Business Sport Life Style Entertainment Travel Sections

Irish News • News Politics Education Health Courts Crime

'What good is an expanded Dart system if Dubliners can't use it?': Plan criticised over lack of new stations



More On Iarnród Éireann - Irish Rail



Revenue at Irish Rail cut in half due to impact of Covid lockdown



Irish Rail to keep...

Privacy

Media Name: The Times

Date: 13th May 2021

Available online at: <https://www.thetimes.co.uk/article/DART-expansion-shoots-past-areas-most-in-need-k3b7dlbq9>

THE TIMES Today's sections ▾ Past six days Explore ▾ Times Radio Log in **Subscribe** Search 🔍

Dart expansion 'shoots past areas most in need'

Sonja Tutty

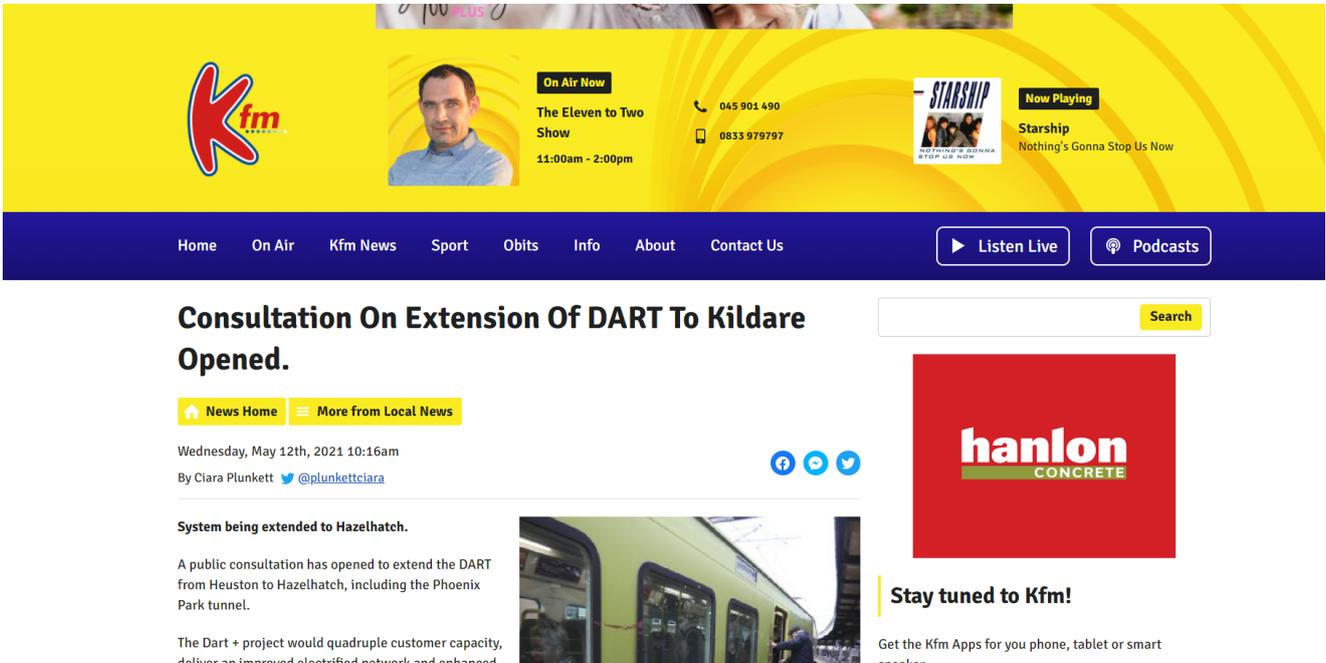
Thursday May 13 2021, 12.01am, The Times



Media Name: KFM

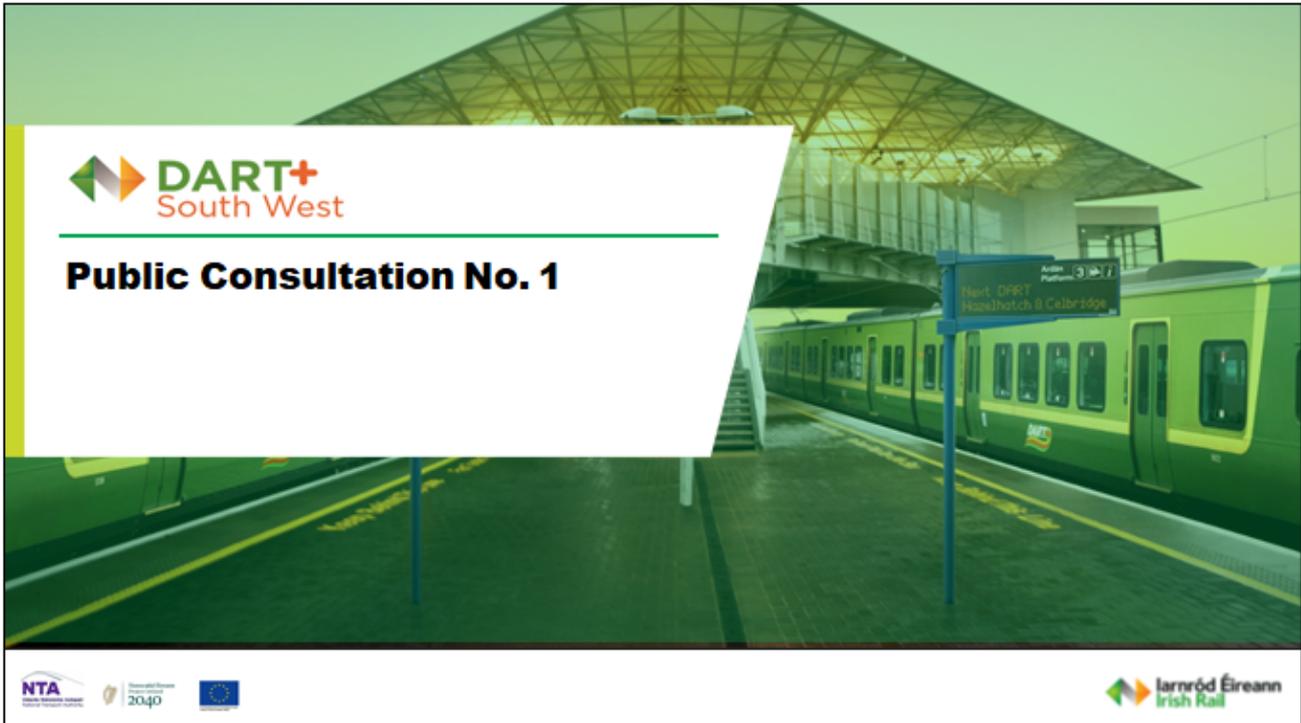
Date: 12th May 2021

Available online at: <https://www.kfmradio.com/news/localnews/consultation-on-extension-of-DART-to-kildare-opened/>



The screenshot shows the KFM website interface. At the top, there is a yellow banner with the KFM logo, a photo of a man, and information about 'The Eleven to Two Show' (11:00am - 2:00pm) and 'Starship' (Nothing's Gonna Stop Us Now). Below the banner is a navigation menu with links for Home, On Air, Kfm News, Sport, Obits, Info, About, and Contact Us. The main content area features the headline 'Consultation On Extension Of DART To Kildare Opened.' with a search bar and social media icons. The article text includes the date 'Wednesday, May 12th, 2021 10:16am' and the author 'By Ciara Plunkett @plunkettciara'. The article content is partially visible, mentioning 'System being extended to Hazelhatch.' and 'A public consultation has opened to extend the DART from Heuston to Hazelhatch, including the Phoenix Park tunnel.' There is also a 'hanlon CONCRETE' advertisement and a 'Stay tuned to Kfm!' section with a call to action to get the Kfm Apps.

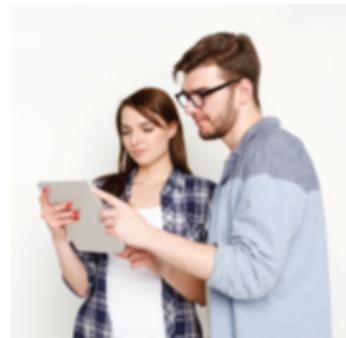
Appendix B. Sample Presentation to Elected Members & Other Stakeholders



1

Why are we here today?

- We have commenced public consultation on the DART+ South West Project
- Consultation will last 6 weeks until 23rd June
- Purpose of this consultation is to seek feedback on the Emerging Preferred Option
- Intention of this briefing is to bring you through the key elements of the Project



2

DART+ Programme – Geographical Extent

- Current DART network extends from Malahide/Howth to Greystones along 50km railway corridor.
- DART+ Programme will increase the extent of electrification to 150km of railway corridor.
- DART+ Programme extends to:
 - DART+ West
 - DART+ South West
 - DART+ Coastal North
 - DART+ Coastal South
- DART+ Programme will increase the frequency of services on the network and will provide high capacity trainsets.



3

Why investment in the DART+ Programme is needed?

Provides Sustainable Transport Options

- Over-reliance on private car use and increasing congestion in Greater Dublin Area.
- DART+ trans are more sustainable and cleaner than current diesel trains.

Achieve Climate Change Targets

- Will help reduce the transport sector greenhouse gas emissions which continue to rise.
- Supporting the Government's Climate Action Plan.



Integration of Land-use & Transport Planning

- Co-ordination and integration of spatial planning with rail transport.
- Supporting compact growth and increased densities in the Greater Dublin Area.
- Supports the implementation of the Project Ireland 2040 and the National Planning Framework.

Facilitates Integration with other modes of transport

- Improves integration of rail services with active modes of travel (walking and cycling).
- Enables greater cross-modal journeys through improved integration with other modes – Bus, Luas, proposed MetroLink and Dublin Bikes.

Supporting Economic and Population Growth

- Congestion in Greater Dublin Area is increasing.
- Cost of Time Lost in the Dublin Region is – €350million/annum and forecast to rise to €2,000million/annum by 2033.
- Sustainable public transport infrastructure (pedestrian, cycling, bus and rail) will sustain economic and population growth while reducing emissions.

4

DART+ Programme – Policy Context

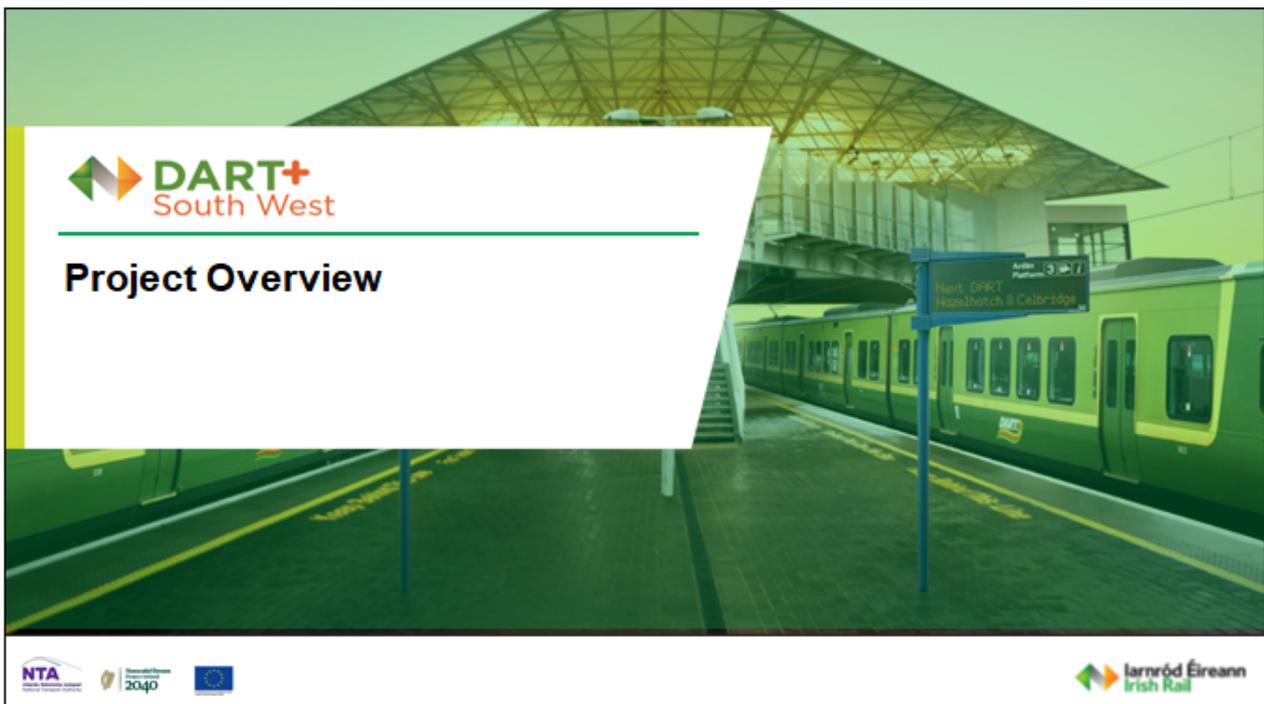
The DART+ Programme is a key deliverable of Ireland's *National Planning Policy* and provides for a number of National Strategic Outcomes of the **National Planning Framework** and priorities of the **National Development Plan (2018-2027)**.

The DART+ Programme supports regional policy identified in the **Regional Spatial and Economic Strategy (RSES)** in the Dublin Metropolitan and Eastern & Midlands Regions.

The National Planning Framework highlights that the delivery of key rail projects identified in the **Transport Strategy for the Greater Dublin Area (GDA) 2016 – 2035** are a key future growth enabler for Dublin.



5




DART+
 South West

Project Overview







6

DART+ South West – Project Route Corridor



7

DART+ South West – Key Elements

The key elements of DART+ South West include:

- Completion of four-tracking from Park West & Cherry Orchard Station to Heuston Station, extending the works completed on the route in 2009.
- Electrification and re-signalling of the line from Hazelhatch & Celbridge Station to Heuston Station and also from Heuston Station to Glasnevin, via the Phoenix Park Tunnel Branch Line, where it will link with the proposed DART+ West Project.
- Undertaking improvements / reconstructions of bridges to facilitate movement of electrified train services.
- Remove constraints along the Phoenix Park Tunnel Branch Line.
- Feasibility report and concept design for a potential new Heuston West Station.

The 'Emerging Preferred Option' will be compatible with future stations at Kylemore and Cabra, although the construction of these stations is not part of the DART+ South West Project.



8

DART+ South West – Benefits



Increase peak passenger capacity from 5,000 to 20,000 per hour per direction and increase train frequency between Hazelhatch & Celbridge Station and Dublin City – facilitating fast, frequent and reliable transport to the surrounding communities.



Enhance public transport opportunities for work, education or leisure purposes.



Facilitate the development and future growth of existing and new communities that will greatly benefit from the connectivity that the DART+ South West will deliver.



Alleviate road congestion.



Build a sustainable and connected city region, supporting the transition to a low carbon and climate resilient society.



Facilitate people to make sustainable travel choices by encouraging a move away from private cars to reliable, efficient and safe public transport network.



Improve multimodal transport connectivity through interchange with the Luas at Heuston Station, Bus Connects and the proposed MetroLink.



Improve journey time reliability.

9

DART+ South West – Capacity Increase



no. of trains
per hour



passenger
capacity

Current

Capacity increases provided by
DART+ South West

Completion

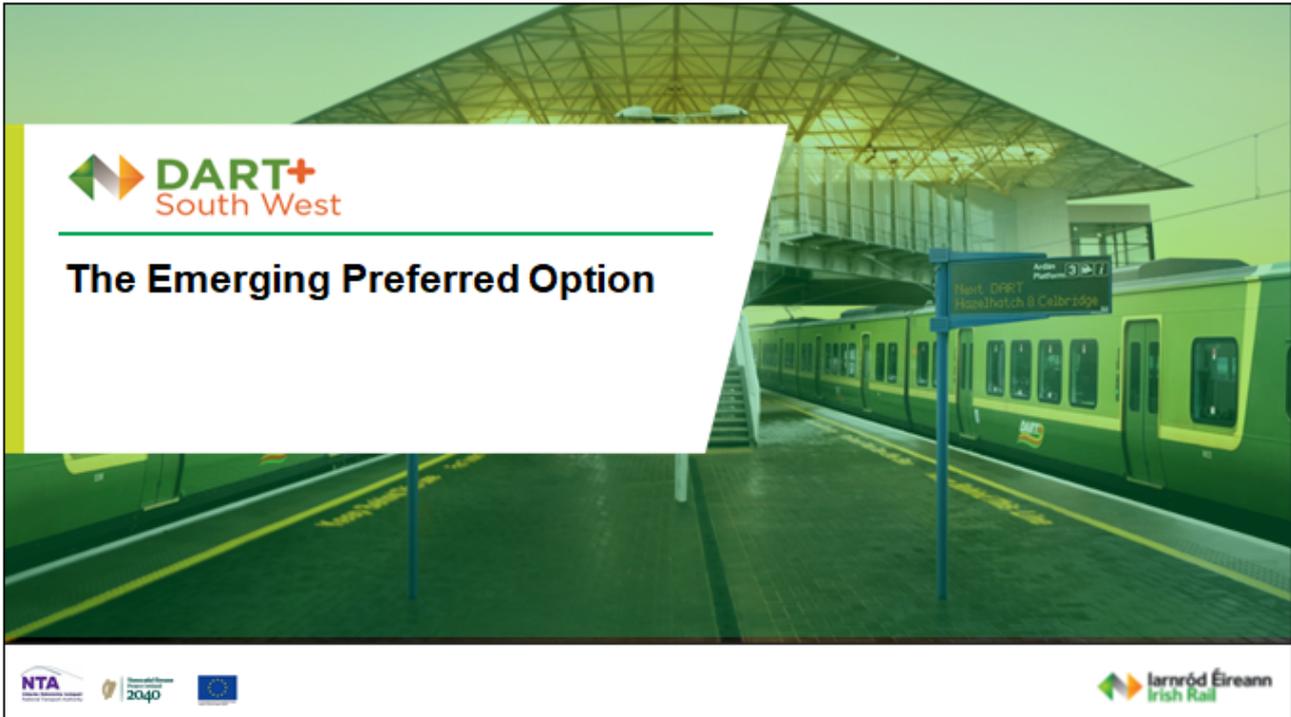


no. of trains
per hour



passenger
capacity

10



11

Option Selection Process

- A structured optioneering process has been followed:
 - Stage 1 – Preliminary Appraisal of Options (Sifting), followed by;
 - Stage 2 – Multi-Criteria Analysis of short-listed options.
- Our starting principle – to upgrade the existing railway corridor and undertake all works, within the railway corridor. This can be achieved over most of the route.
- The last remaining significant constraint is from the Park West area to Heuston Station where the rail corridor currently reduces to two tracks.

```

            graph TD
            A[Stage 1: Preliminary Assessment (Sifting)  
(Long list of options)] --> B[Stage 2: Multi-Criteria Analysis  
(Feasible Options)]
            B --> C[Emerging Preferred Options  
(Specific Interventions for  
Four Tracking and at Structure)]
            D[General Linear Works] --> E[Emerging Preferred Option]
            C --> E
            E --> F[Public Consultation No. 1]
            
```

12

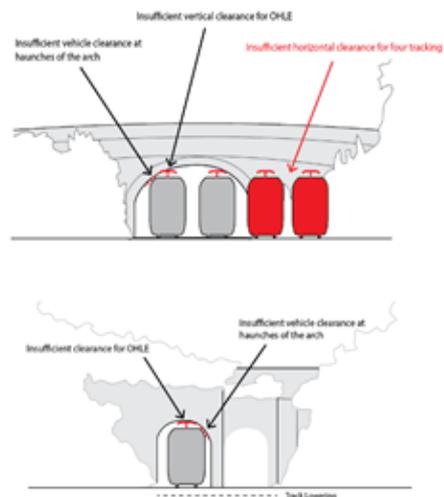
Emerging Preferred Option – General Linear Works

- Overhead electrification equipment along the full extent of the railway line. This will be similar to that currently used on the existing DART network.
- Overhead electrified line protection works at all existing rail overbridges.
- Construction of a number of electrical substations at intervals along the rail line, to provide power to the network.
- Signalling upgrades and additional signalling to the upgraded infrastructure.
- Interfaces with existing utilities, boundary treatments (including new retaining walls), drainage works, vegetation management and other ancillary works along the length of the project.



Emerging Preferred Option – General Linear Works

- Where existing bridges do not provide the necessary vertical clearance for overhead electrification of the lines or lateral clearance for four tracking, options are being considered on a case-by-case basis, these include:
 - Use of specialist overhead electrical equipment with reduced clearance;
 - Lowering the rail track under the bridge;
 - Modification of the existing structure;
 - Removal of the existing structure and delivery of a replacement structure; or
 - A combination of the above.



Emerging Preferred Option

Four Tracking Area



Existing Four Track System

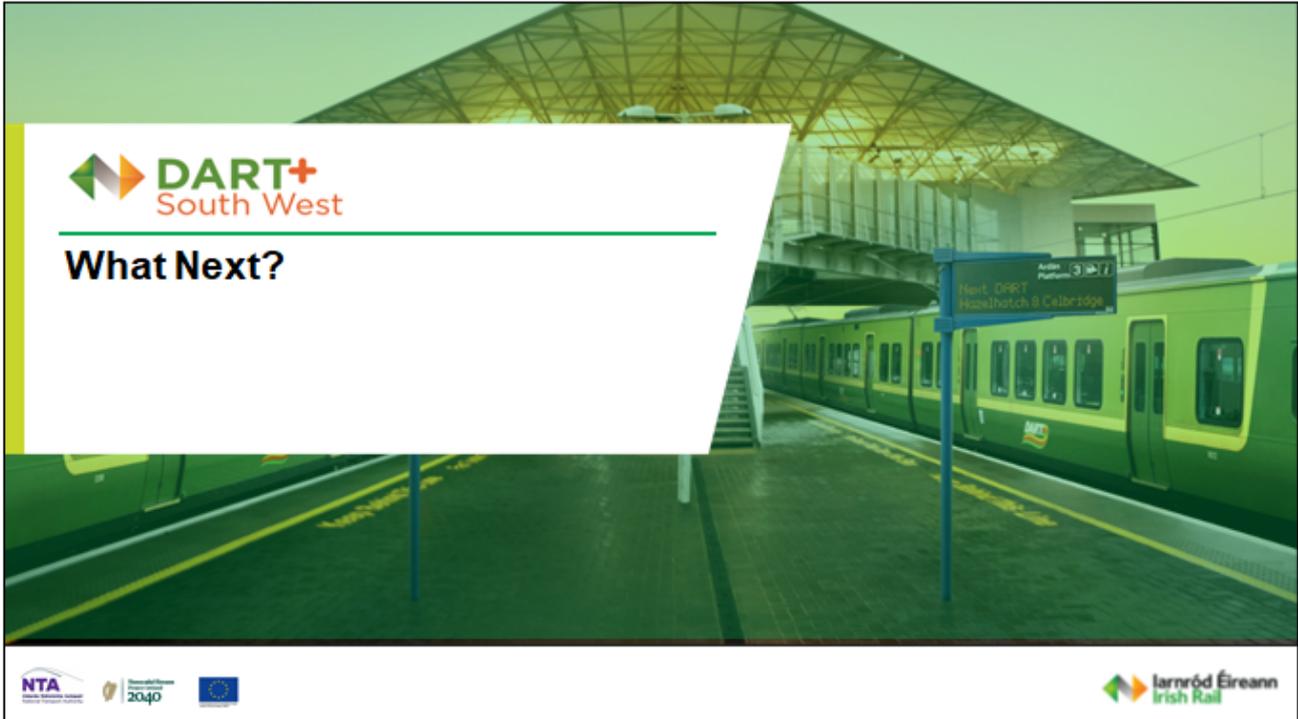
- Completion of the four tracking from Park West & Cherry Orchard Station to Heuston Station will require reconstruction of the existing bridges in the area, to provide the necessary width to accommodate the expanded railway corridor and height for overhead electrification equipment.
- The bridges are:
 - Le Fanu Road Bridge
 - Kylemore Road Bridge
 - Khyber Pass Footbridge
 - Sarsfield Road Underbridge (deck)
 - Memorial Road Bridge
- In addition, a new structure will be constructed to the north of the South Circular Road Bridge. The new structure would be for the new DART tracks and the existing Intercity service would continue under the South Circular Road Bridge.

Emerging Preferred Option

Phoenix Park Tunnel Branch Line

- DART+ South West is currently undertaking surveys and analysis along this section, including within the tunnel, to understand the current characteristics and constraints.
- The Emerging Preferred Option will follow the existing rail corridor and may involve track lowering and / or bridge modifications at certain locations to achieve the height requirements for electrification.
 - The bridges are:
 - Conyngham Road Overbridge,
 - McKee Barracks Bridge
 - Blackhorse Avenue Bridge
 - Old Cabra Road Bridge
 - Cabra Road Bridge
 - Faussagh Road Bridge
 - Royal Canal and LUAS Twin Arch
 - Maynooth Line Twin Arch
 - Glasnevin Cemetery Road Bridge





17

Public Consultation

We have commenced Public Consultation No. 1 on the Emerging Preferred Option.

We are asking the public, as potential users of the improved services, and those likely to be affected by its development, on their views of our plans to inform the emerging preferred design and help us improve the project.

As the design process advances and the designs are further developed and matured, another public consultation will take place.



18

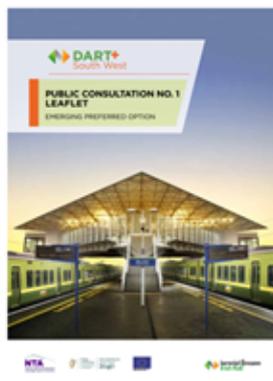
Public Consultation Documentation

- Letter to potentially affected landowners
- Leaflet to adjacent communities
- Virtual consultation room
 - Multi-media engagement tools
- Dedicated website - www.dartplus.ie
 - Project documentation
 - Brochure
 - Preliminary Options Selection Report
 - Technical documentation
- Iarnród Éireann is hosting a number of public engagement webinars for communities along the route.



19

Public Consultation Documentation



20

How People can Engage



Contact via the following means:

Website | www.dartplus.ie

Email | DARTSouthWest@irishrail.ie

Phone line | (01) 284 1029

Or in writing to:

Community Liaison Officer,
 DART+ South West,
 Iarnród Éireann,
 Inchicore Works,
 Inchicore Parade,
 Dublin 8.
 D08K6Y3

21

Thank you



22

Appendix C. PC1 Leaflet and Brochure



PUBLIC CONSULTATION NO. 1 LEAFLET

EMERGING PREFERRED OPTION



DART+ Programme

The DART+ Programme is a transformative railway investment programme, that will modernise and improve the existing rail services in the Greater Dublin Area. It will provide a sustainable, electrified, reliable and more frequent rail service, improving capacity on the rail corridors serving Dublin. The current electrified DART network is 50km long, extending from Malahide / Howth to Bray / Greystones and the DART+ Programme seeks to increase the electrified network to 150km.

The DART+ Programme is required to facilitate increased train capacity to meet current and future demands which will be achieved through a modernisation of the existing railway corridors.

It will also contribute to Ireland's transition to a low carbon and climate resilient society. The DART+ Programme comprises the following improvement projects across the four main rail corridors:

- DART+ West - Maynooth and M3 Parkway to the City Centre.
- DART+ South West - Hazelhatch & Celbridge to the City Centre.
- DART+ Coastal North - Drogheda to the City Centre.
- DART+ Coastal South - Greystones to the City Centre.
- DART+ Fleet - purchase of new train fleet to increase train services.



DART+ South West

The DART+ South West Project, as part of the DART+ Programme, will deliver an improved electrified network, with increased passenger capacity and enhanced train service between Hazelhatch & Celbridge Station and Heuston Station (c. 16km) on the Cork Mainline, and to Glasnevin via the Phoenix Park Tunnel Branch Line (c. 4km).

DART+ South West will significantly increase train capacity from the current 12 trains per hour per direction to 23 trains per hour per direction (i.e. maintain the existing 12 services, with an additional 11 train services provided by DART+ South West).



This will increase passenger capacity from the current peak capacity of approximately 5,000 passengers per hour per direction to approximately 20,000 passengers per hour per direction, as the new fleet being delivered as part of the DART+ Programme will have greater carrying capacity.

The DART+ South West Project is now launching a public consultation on the **'Emerging Preferred Option'**, which is the combined end-to-end design options identified at this early stage of the project development.

Benefits of DART+ South West



Increase peak passenger capacity from 5,000 to 20,000 per hour per direction and increase train frequency between Dublin City and Hazelhatch & Celbridge Station – facilitating frequent and reliable transport to the surrounding communities.



Enhance public transport opportunities for work, education or leisure purposes.



Facilitate the development and future growth of existing and new communities that will greatly benefit from the connectivity that the DART+ South West will deliver.



Alleviate road congestion.



Build a sustainable and connected city region, supporting the transition to a low carbon and climate resilient society.



Facilitate people to make sustainable travel choices by encouraging a move away from private cars to a reliable, efficient and safer public transport network.



Improve multimodal transport connectivity through interchange with the Luas at Heuston Station, Bus Connects and the proposed MetroLink.



Improve journey time reliability.

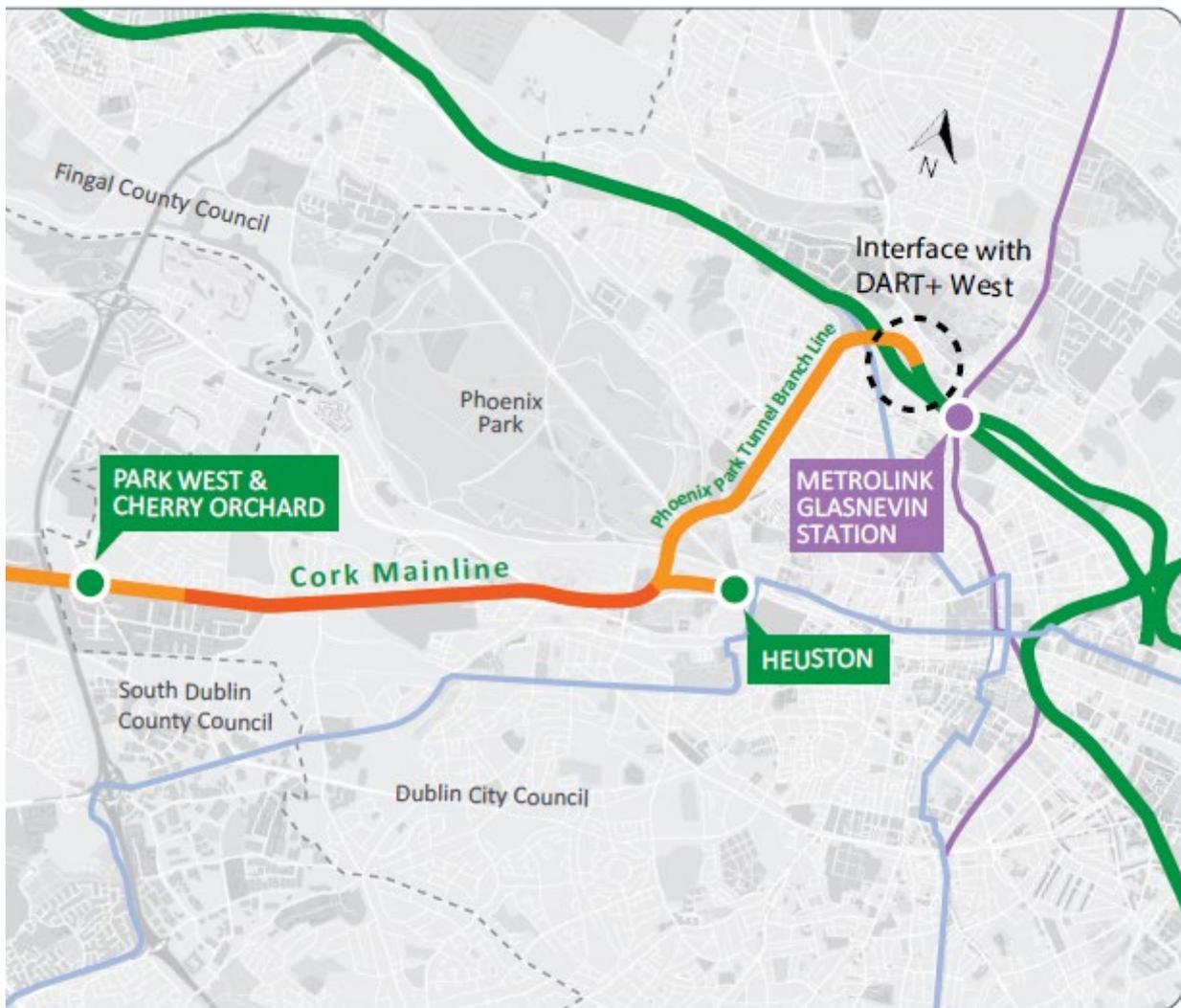




Scope of DART+ South West

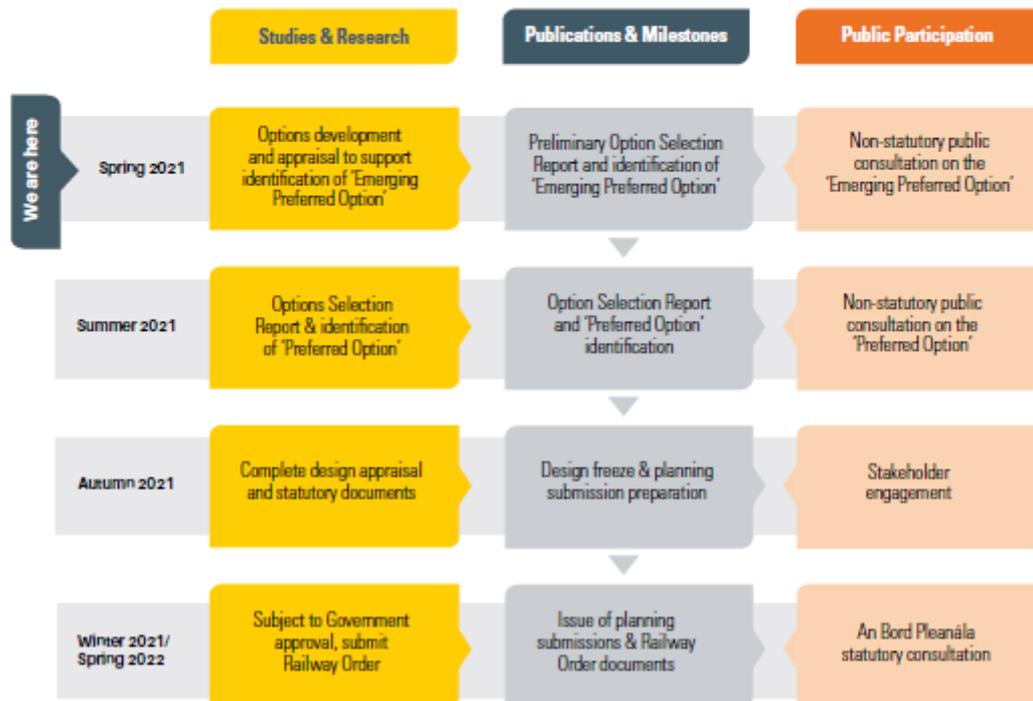
The following is a high-level summary of the key elements of DART+ South West:

- Completion of four tracking, widening the rail corridor, from Park West & Cherry Orchard Station to Heuston Station, extending the works completed on the route in 2009.
- Electrification and re-signalling of the line from Hazelhatch & Celbridge Station to Heuston Station and also from Heuston Station to Glasnevin, via the Phoenix Park Tunnel Branch Line, where it will link with the proposed DART+ West.



- Undertaking improvements/reconstructions of bridges to facilitate movement of electrified train services.
- Remove rail constraints along the Phoenix Park Tunnel Branch Line.
- Feasibility report and concept design for a potential new Heuston West Station.
- The **'Emerging Preferred Option'** will be compatible with future stations at Kylemore and Cabra, although the construction of these stations is not part of the DART+ South West Project.

Timeline



How to Engage

As users of the service, or those likely to be affected by its development, the public is invited to contribute to this consultation process to express opinions on the DART+ South West Emerging Preferred Option. The consultation period is now open, full details are available on the project website.

You can engage with us via the following means:

Website | www.dartplus.ie

Email | DARTSouthWest@irishrail.ie

Phone line | (01) 284 1029

Postal Address

If you would prefer to write to us, please send it or any correspondence to:

Community Liaison Officer

DART+ South West

Iarnród Éireann

Inchicore Works

Inchicore Parade

Dublin 8

D08K6Y3


 The EU is part funding the DART+ South West design. The contents of this publication are the sole responsibility of Iarnród Éireann and do not necessarily reflect the opinion of the European Union.



TABLE OF CONTENTS

| | | |
|-----------|---|-----------|
| 1 | Introduction to DART+ Programme | 1 |
| 2 | DART+ South West | 6 |
| 3 | Public Consultation Process | 8 |
| 4 | Current Design Status | 10 |
| 5 | Key Infrastructural Elements of DART+ South West | 12 |
| 6 | Benefits of DART+ South West | 16 |
| 7 | Option Selection Process | 19 |
| 8 | The Emerging Preferred Option | 21 |
| | 8.1 General Linear Works | 22 |
| | 8.2 Hazelhatch & Celbridge Station to Park West & Cherry Orchard Station | 23 |
| | 8.3 Park West & Cherry Orchard Station to Heuston Station | 23 |
| | 8.4 East of South John's Road Bridge to Glasnevin Junction | 26 |
| 9 | Transport Integration | 28 |
| 10 | Issues to Consider | 30 |
| 11 | Next Steps | 33 |
| 12 | How to Engage | 35 |
| 13 | Alignment Figures | 37 |





1. Introduction to DART+ Programme

The current DART network is 50km long, extending from Malahide / Howth to Greystones. The DART+ Programme will increase the length of the DART network to 150km of railway corridor through the electrification and upgrade of existing lines transforming commuter train travel in the Greater Dublin Area (GDA). The DART+ Programme also includes the purchase of new train fleet. The DART+ Programme will deliver frequent, modern, electrified services from Dublin City Centre to:

- Maynooth, M3 Parkway
- Hazelhatch & Celbridge
- Drogheda; and
- Greystones

DART+ Programme is a key transportation improvement to form a high quality and integrated public transport system. It will have benefits for the residents of the Greater Dublin Area and also those living in the other regions. It will assist in providing a sustainable transport system and a societal benefit for current and future generations.

DART+ Programme will seek to maximise use of the existing railway corridors and implement a modernisation programme to achieve the capacity increase necessary to meet current and future demands.





Schematic diagram of DART+ Programme extent

03

Why investment in DART+ Programme is needed

Provides Sustainable Transport Options

- Over-reliance on private car use and increasing congestion in Greater Dublin Area.
- DART trains are more sustainable and cleaner than current diesel trains.

Achieve Climate Change Targets

- Will help reduce the transport sector greenhouse gas emissions which continue to rise.
- Supporting the Government's Climate Action Plan.



Supporting Economic and Population Growth

- Congestion in Greater Dublin Area is increasing.
- Cost of Time Lost in the Dublin Region is - €350million/annum and forecast to rise to €2,000million/annum by 2033.
- Sustainable public transport infrastructure (pedestrian, cycling, bus and rail) will sustain economic and population growth while reducing emissions.

Integration of Land-use & Transport Planning

- Co-ordination and integration of spatial planning with rail transport.
- Supporting compact growth and increased densities in the Greater Dublin Area.
- Supports the implementation of the Project Ireland 2040 and the National Planning Framework.

Facilitates Integration with other modes of transport

- Improves integration of rail services with active modes of travel (walking and cycling).
- Enables greater cross-modal journeys through improved integration with other modes - Bus, Luas, proposed MetroLink and Dublin Bikes.

04





2. DART+ South West

The second of the infrastructural projects of the DART+ Programme to be delivered will be the DART+ South West Project.

DART+ South West is seeking to significantly increase rail capacity on the Cork Mainline between Hazelhatch & Celbridge Station to Heuston Station, and to Glasnevin via the Phoenix Park Tunnel Branch Line. This can be achieved by implementing an electrified railway network with high capacity DART trains and increasing the frequency of trains.

Delivery of this project will support existing communities along the railway and support future sustainable development. It will serve all existing stations along the railway corridor between Hazelhatch & Celbridge Station to Heuston Station and Heuston Station to Glasnevin using electrical power that has a lower carbon footprint than the existing diesel trains. The frequency and quality of service will provide a viable transport alternative to communities along the route and help encourage people to migrate from private car use. This will assist Ireland in reducing greenhouse gas emissions from transport and help combat climate change.

The electrification of the rail line will predominantly follow the existing railway corridor. Works outside of Iarróid Éireann lands will be required at a number of locations for some of the scheme elements such as:

- Widening of the railway corridor for four-tracking between Park West & Cherry Orchard Station and Heuston Station;
- Bridge reconstruction and/or improvements;
- Construction of substations (to facilitate the provision of power to the line); and
- Use of land for temporary construction/storage compounds and all ancillary works required for the project.





3. Public Consultation Process

Public participation during the design process is a key element to the delivery of major infrastructure projects such as DART+ South West.

This project has a two stage non statutory Public Consultation process. This current consultation is seeking feedback on the **'Emerging Preferred Option'**. As the design process advances and the designs are further developed and matured another public consultation will take place.

Public Consultations are our way of asking you, as potential users of the Improved services or those likely to be affected by its development, for your views on our plans, whilst the design process is active. Your local knowledge and comments will inform the emerging preferred design and help us improve the project and ensure it will be a success for you and the communities it will serve.

Public participation is welcomed and encouraged throughout the design development process, which will provide you with the opportunity to learn about the design as it develops and provide feedback which will inform the next stage as appropriate. The

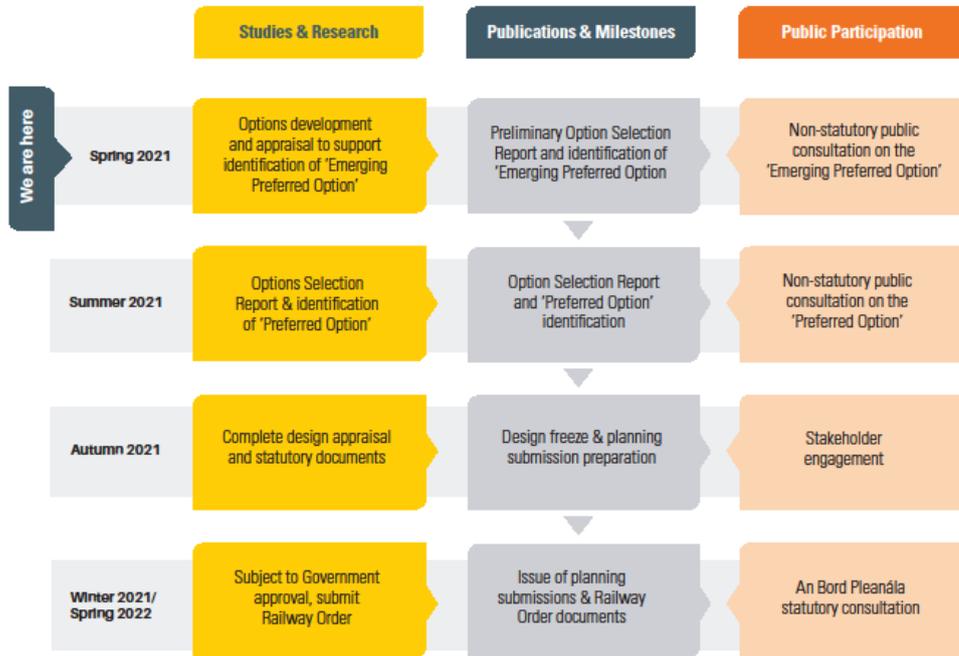
main public participation/feedback stages as part of the project development are illustrated in graphical form below and include:

- Public Consultation No. 1 on the Emerging Preferred Option (Spring 2021) - **Current stage**
- Public Consultation No. 2 on the Preferred Option (Summer 2021)
- Statutory Consultation Period as part of the Railway Order application process (Winter 2021/Spring 2022)

Public feedback will be accepted during all stages of the design development and can be submitted through the project website, e-mail address, phone line or by written correspondence. For further details see the **'How to Engage'** section.

Iamrúid Éireann invites the public to engage in the design process and all feedback is welcome.

COVID 19 Due to COVID-19 restrictions the first consultation on the **'Emerging Preferred Option'** will be a predominantly digital online public consultation. If COVID-19 restrictions ease further into 2021, the second public consultation event, scheduled for the Summer of 2021, presenting the **'Preferred Option'** to the public may be possible to hold in a physical location.



Graphic showing public participation as part of the option selection, design and Railway Order application process

09



4. Current Design Status

This brochure explains the current design status of the project, its benefits, potential impacts, and how you can send us your queries, thoughts and ideas.

The design and environmental impact assessment process for the DART+ South West has commenced, and we are at a key early stage in the project. DART+ South West has defined an 'Emerging Preferred Option' and we would like to canvass public opinion on this design.

Before we proceed any further, we would like your views on the DART+ South West 'Emerging Preferred Option' which is being put forward by Iarnród Éireann as part of this Public Consultation process No. 1.

The 'Emerging Preferred Option' is the preferred combination of design options that have been identified for each of the elements of the project at this stage of the project development.

Studies are still ongoing in this regard and therefore some site-specific design aspects have yet to be concluded. These studies will be progressed with your local knowledge and will inform the design and help us to improve the project and ensure it will be a success for you and the communities the project will serve.

Following these further studies, assessments, design development and a review of your feedback, the 'Emerging Preferred Option' will be refined, and the 'Preferred Option' will be presented at Public Consultation No. 2 due to take place later in 2021.

The project will culminate with a Railway Order application to An Bord Pleanála, in accordance with the Transport (Railway Infrastructure) Act 2001 (as amended). This is essential to secure building consent. It is currently anticipated that the Railway Order application will be submitted to An Bord Pleanála for approval in later 2021 / early 2022.

Your participation and feedback are an essential part of this stage in the design and assessment process.



11



5. Key Infrastructural Elements of DART+ South West

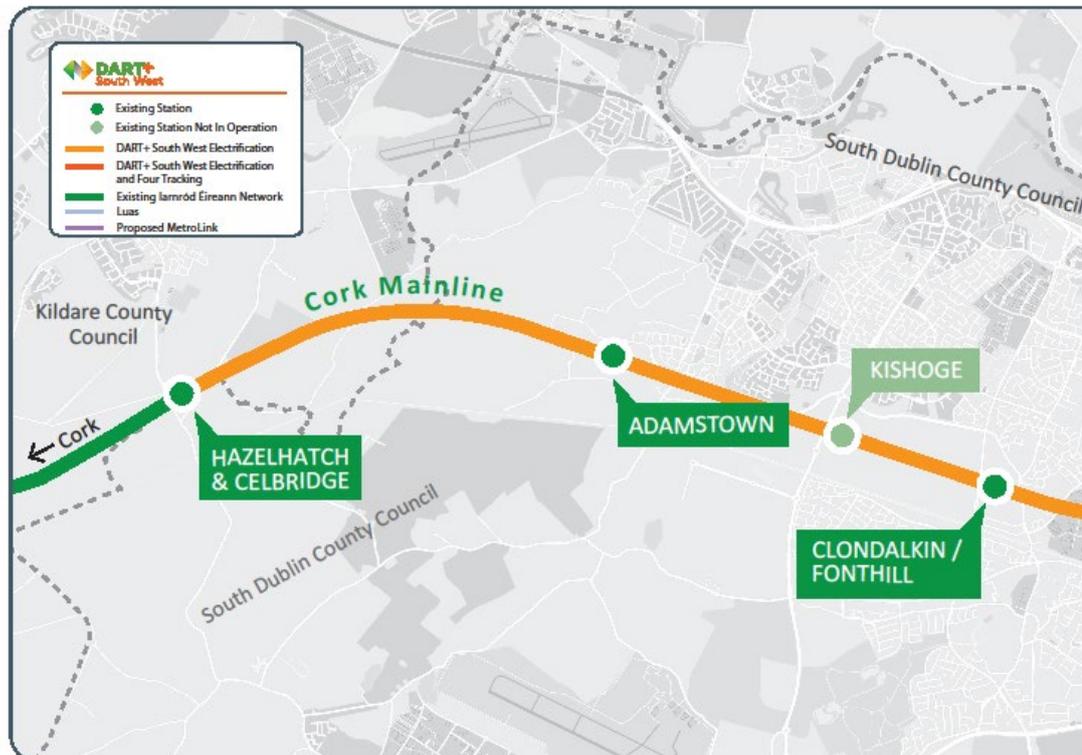
The following is a high-level summary of the key infrastructural elements of the DART+ South West Project:

- Completion of four-tracking from Park West & Cherry Orchard Station to Heuston Station, extending the works completed on the route in 2009.
- Electrification and re-signalling of the line from Hazelhatch & Celbridge Station to Heuston Station and also from Heuston Station to Glasnevin, via the Phoenix Park Tunnel Branch Line, where it will link with the proposed DART+ West
- Undertaking improvements/reconstructions of bridges to facilitate movement of electrified train services.
- Remove rail constraints along the Phoenix Park Tunnel Branch Line.
- Feasibility report and concept design for a potential new Heuston West Station.

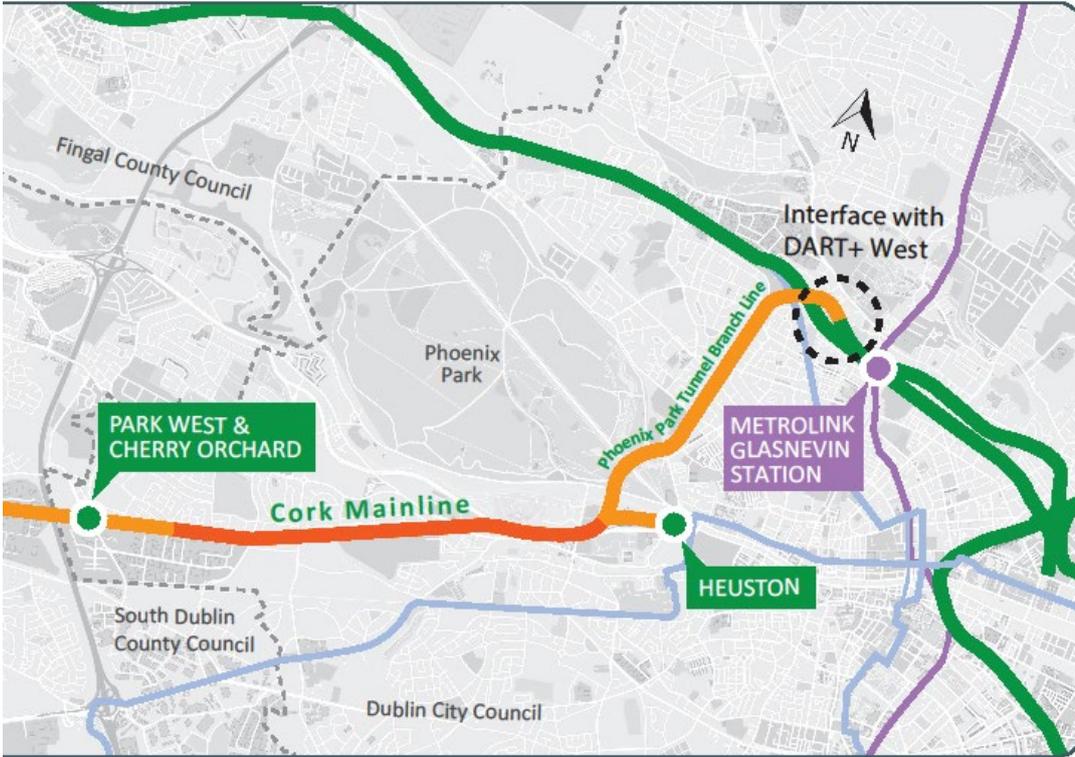
- The 'Emerging Preferred Option' will be compatible with future stations at Kylemore and Cabra, although the construction of these stations is not part of the DART+ South West Project.



13



14 Map of proposed DART+ South West



15



Benefits of DART+ South West



Increase peak passenger capacity from 5,000 to 20,000 per hour per direction and increase train frequency between Hazelhatch & Celbridge Station and Dublin City – facilitating fast, frequent and reliable transport to the surrounding communities.



Enhance public transport opportunities for work, education or leisure purposes.



Facilitate the development and future growth of existing and new communities that will greatly benefit from the connectivity that the DART+ South West will deliver.



Alleviate road congestion.



Build a sustainable and connected city region, supporting the transition to a low carbon and climate resilient society.



Facilitate people to make sustainable travel choices by encouraging a move away from private cars to reliable, efficient and safe public transport network.



Improve multimodal transport connectivity through interchange with the Luas at Heuston Station, Bus Connects and the proposed MetroLink.



Improve journey time reliability.

18



07

Option Selection
Process

7. Option Selection Process

To assist the design development process and to consider various option to determine the **'Emerging Preferred Option'** for DART+ South West, a structured optioneering process has been followed:

- Stage 1 – Preliminary Appraisal of Options (Sifting) followed by
- Stage 2 – Multi-Criteria Analysis of short-listed options.

This structured process evaluates a number of different options and it's based on 'Guidelines on a Common Appraisal Framework for Transport Projects and Programmes' (CAF) published by the Department of Transport March 2016 (updated 2020), TII's Project Management Guidelines (Transport Infrastructure Ireland's Project Management Guidelines 2019) and Iarnród Éireann's Project Approval Guidelines.

Development of Options

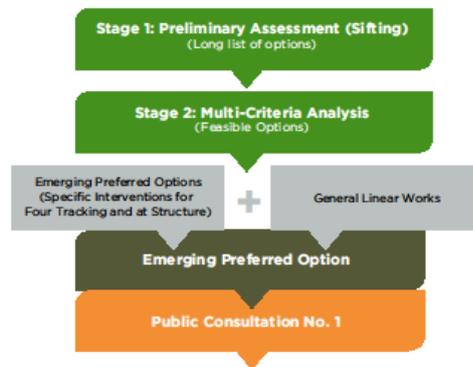
The engineering design will enhance the existing railway network to meet train capacity requirements to cater for current and future projected passenger demand. Many elements of the scheme require option assessment at a local level prior to incorporation into the end to end route determination assessment. Options were developed for the individual components to include the following:

- Four-tracking (involving track enhancement, bridge improvements/reconstructions and other civil works to facilitate movement of electrified train services).

Assessment Methodology

Stage 1 – Preliminary Assessment (sifting process) comprised of the assessment of a long list of options against engineering, economics and environment criteria to evaluate the 'feasibility' of each options to meet the project objectives / requirements. This approach allowed for the long list of options to be filtered to a shorter list of feasible options. All feasible options were brought forward to Stage 2 where they could be explored in greater detail.

Stage 2 – The Multi-Criteria Analysis process comprised of a more detailed multi-disciplinary comparative analysis of the feasible options which passed through Stage 1. The feasible options were assessed against the six appraisal criteria set out in the NTA's Common Appraisal Framework (CAF), namely: economy, safety, environment, accessibility and social inclusion, integration and physical activity. Options were then compared to each other based on whether an option had a 'slight' or 'significant' advantage or disadvantage over other options or whether all options were 'comparable / neutral' leading to the determination of emerging preferred options for the intervention required. The various emerging preferred options in respect of particular elements or interventions were then combined with general linear works needed to upgrade and modernise the railway to make up the end-to-end **'Emerging Preferred Option'**.



Graphic showing the assessment methodology



8. The Emerging Preferred Option

The starting principle for the project is to upgrade the existing railway corridor and undertake all works, within the railway corridor. This can be achieved over the majority of the route, including building on the groundwork carried out under the original Kildare Route Project which delivered the existing four track system and several reconstructed bridges from Hazelhatch & Celbridge Station to Park West & Cherry Orchard Station. The last remaining significant constraint is from Park West area to Heuston Station where the rail corridor reduces to two tracks. Expanding to four tracks will require widening of the rail corridor and this will have a potential impact on adjoining property owners.

The process to determine the 'Emerging Preferred Option', as described in Section 7 - Option Selection Process, has led to the identification of Emerging Preferred Options in respect of works required. These, and general linear works required along the full length of the project, are the key elements of the Emerging Preferred Option.

For the purpose of describing the Emerging Preferred Option, general linear works are described first followed by sections (from west to east) with similar project requirements and resulting levels of works or interventions, as follows:

- General Linear Works.
- Hazelhatch & Celbridge Station to Park West & Cherry Orchard Station.
- Park West & Cherry Orchard Station to Heuston Station.
- East of St John's Road Bridge to Glasnevin Junction.

8.1 General Linear Works

The project will require modernisation and modifications to the existing railway line. There is a range of general linear works required along the full length of the project to enable the electrification of the line and the upgrade of the existing network. These are:

- Overhead electrification equipment (OHLE) will be required along the full extent of the railway line from Hazelhatch & Celbridge Station to Heuston Station and through the Phoenix Park Tunnel Branch Line up to Glasnevin Junction, where it will link with the proposed DART+ West Project. This will be similar to the OHLE currently used on the existing DART network.
- A number of electrical substations will be required at intervals along the rail line

to provide power to the network.

- Signalling upgrades and additional signalling will be required to the upgraded infrastructure.
- Where existing bridges do not provide the necessary height for overhead electrification of the lines or width for four tracking, options are being considered on a case-by-case basis, these include:
 - Provision of specialist electrical solutions for the OHLE with reduced clearance;
 - Lowering the rail track under the bridge;
 - Modification of the existing structure;
 - Removal of the existing structure and provision of a replacement structure; or
 - A combination of the above.
- Overhead electrified line protection works will be required at all existing rail overbridges.
- Interfaces with existing utilities, boundary treatments (including new retaining walls), drainage works, vegetation management and other ancillary works will be required along the length of the project.



View of typical section of twin track electrified rail line

22

8.2 Hazelhatch & Celbridge Station to Park West & Cherry Orchard Station

The works carried out under the original Kildare Route Project between 2006 and 2009 provided the main groundwork for DART+ South West including the existing four track system and several reconstructed bridges.

The Emerging Preferred Option for this circa 11km section comprises the general linear works as outlined in Section 8.1. The electrification works can be run under the existing bridges with no / minimal intervention in the bridge structures and minor localised track lowering works and use of specialist OHLE solutions to achieve the required clearance. All these works can be accommodated within the existing rail corridor.



Existing Four Track System

8.3 Park West & Cherry Orchard Station to Heuston Station

The section between Park West & Cherry Orchard Station and Heuston Station requires electrification and widening to four tracks. To meet these project requirements, the track corridor must be widened, and the physical surroundings must be altered. Extending to four tracks in this area will require an increase in the width of the existing rail corridor outside of lands owned by Iarróid Éireann, potentially interfering with property rights (on a permanent and / or temporary basis).

Following an option selection process that included developing and evaluating a number of options at each location, the Emerging Preferred Option for each location was established. These are described below:

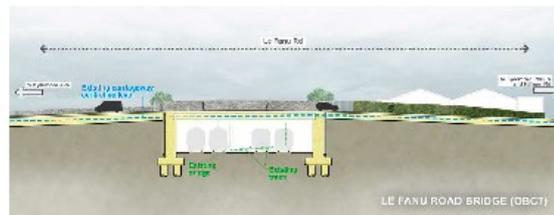
8.3.1 Area around Le Fanu Bridge

The rail corridor on the Cork Mainline between Cherry Orchard Footbridge and Le Fanu Road Bridge currently comprises three existing tracks and at Le Fanu Road Bridge narrows to two existing tracks. Increasing to four tracks requires the realignment of the existing tracks and an increase in the overall railway corridor width. Le Fanu Road Bridge is a narrow arch structure and is inadequate in both span length and height for the four tracks and electrification infrastructure.

The Emerging Preferred Option replaces the bridge with a longer span or spans to facilitate the additional width required for the additional tracks. To overcome the lack of height available for the electrification infrastructure, the road level will be raised in combination with lowering the rail track. Retaining walls are required to the north and south of the corridor adjacent to the new bridge to allow the widening of the corridor while minimising the impact on the adjacent properties. The raising of the road level will also mean that retaining walls will be required along the road to the north of the railway.

The proposed replacement bridge will be a modern structure that will provide segregation for pedestrians, cyclists and improved sightlines and will be a significant improvement on the existing situation for all road users.

The proposed new bridge is presented below in sectional elevation looking east.

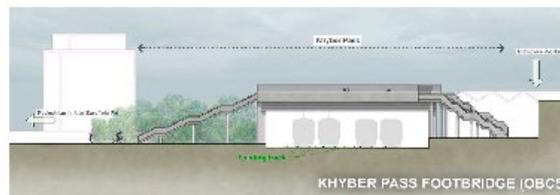


Emerging Preferred Option for Le Fanu Road Bridge

23



Emerging Preferred Option for the Kylemore Road Bridge



Emerging Preferred Option for Khyber Pass Footbridge

8.3.2 Area around Kylemore Road Bridge

This section of the railway comprises two existing tracks and one bridge structure (Kylemore Road Bridge). The bridge does not have adequate span length to fit four tracks and is not high enough for the DART line electrification infrastructure to pass under. There are a number of constraints in this area including:

- The railway corridor is bounded on both sides by soil slopes.
- To the north and south of the bridge are road junctions and access points that significantly restrict alterations that may be required to the road geometry.
- Kylemore Road is a potential route for a future LUAS line. Therefore, the design must consider this potential new infrastructure.
- The west of Kylemore Road Bridge has been identified for a potential future railway station to the west of the bridge. The designs for this area must not prejudice its delivery in the future.

The Emerging Preferred Option for Kylemore Road Bridge replaces the bridge with a longer span to facilitate the additional track width. To overcome the lack of height available for the electrification infrastructure, the road level will be raised in combination with lowering the rail track.

Retaining walls are required to the north and south of the corridor to allow the widening of the corridor while minimising the impact on the adjacent properties. The raising of the road level will also mean that retaining walls will be required along the road to the north and south of the railway.

The proposed new bridge is presented above in sectional elevation looking east.

8.3.3 Area around Inchicore Works

The railway in this area (between Kylemore Road Bridge and Sarsfield Road Bridge) comprises two mainline tracks which are joined by two additional short tracks (or sidings) connected to the Inchicore Depot. The existing tracks through the area would not provide the required four tracking while maintaining the functionality of the depot. Therefore, the laying of additional tracks is required, which in turn requires the realignment of the existing tracks and an increase in the railway corridor width in this area.

The Emerging Preferred Option focuses this enhancement of the corridor to the south requiring the demolition / modification of some lamróid Éireann facilities within the Inchicore Depot. There is potential interference to third party property rights but further design development and technical and construction related solutions will seek to minimise this.

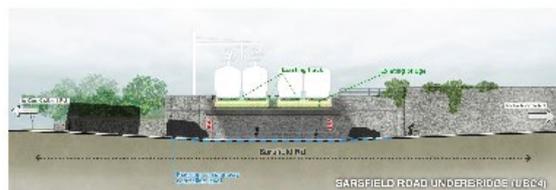
8.3.3.1 Khyber Pass Footbridge

Khyber Pass Footbridge is an existing pedestrian overbridge linking Inchicore Works to Sarsfield Road to the north. The existing structure has three tracks beneath it and is not wide enough to safely accommodate an increase to four tracks.

The Emerging Preferred Option provides a new pedestrian bridge with sufficient height and width to meet the requirements for four-tracking and electrification. The extent of works may potentially interfere with property rights in the immediate area but further design development and technical and construction related solutions will seek to minimise this.

The proposed new pedestrian bridge is presented above in sectional elevation looking east towards Heuston Station.

24



Emerging Preferred Option for Sarsfield Road Bridge

8.3.4 Sarsfield Road Bridge Area

Sarsfield Road Bridge carries the railway over Sarsfield Road. Both the bridge and the railway corridor in this area comprise three mainline tracks and are not wide enough to carry the fourth track that is required.

The Emerging Preferred Option replaces the existing bridge deck with two parallel bridge decks, one for the Intercity service and one for the DART service. The existing walls along Sarsfield Road would be mostly left untouched by the construction works. The proposed bridge is presented above in sectional elevation looking east towards Heuston Station.

There is potential interference to third party property rights but further design development and technical and construction related solutions will seek to minimise this. Heading east of the bridge the corridor will predominantly be widened to the north to add a fourth track (into the embankment between the railway and Con Colbert Road).



Emerging Preferred Option for Memorial Road Bridge

8.3.5 Area around Memorial Road Bridge

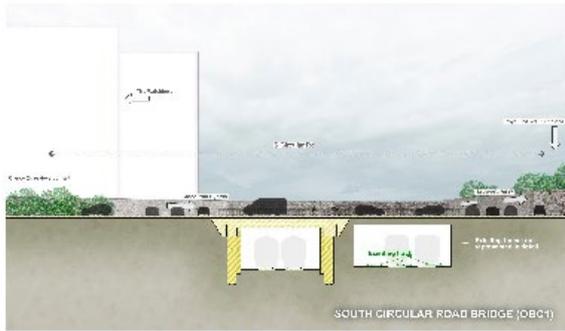
The existing Memorial Road Bridge is too short in span length to accommodate the additional fourth track, so a longer span bridge is required. The existing bridge also does not have the height required to accommodate the electrification infrastructure beneath the bridge. The bridge is very close to the Con Colbert dual carriageway so any increases in the height of the road would have an impact on the dual carriageway.

The Emerging Preferred Option replaces the bridge with a longer span bridge. In addition, the rail tracks will be lowered to facilitate the electrification infrastructure beneath the new bridge. The masonry retaining walls on the southern side would need to be strengthened due to the lowering of the track and new retaining walls would be required along the northern side.

The permanent way boundary wall along Con Colbert Road will need to be reconstructed to a higher containment standard and height, as it will be removed to provide retaining wall construction access.

The proposed bridge is presented above in sectional elevation looking east towards Heuston Station.

25



Emerging Preferred Option for South Circular Road Bridge



Aerial View of Emerging Preferred Option for South Circular Road Junction

8.3.6 South Circular Road Junction Area

This area extends from Memorial Road Bridge to the South Circular Road Junction. There are two major bridge structures in this area which are part of the junction namely South Circular Road and St. John's Road Bridge. St. John's Road Bridge has an adequate span length to enable a layout with the minimum four tracks requirement and is high enough for the electrification infrastructure required for DART. South Circular Road Bridge does not have adequate span length to fit four tracks and is not high enough for the electrification infrastructure to pass under.

The Emerging Preferred Option leaves South Circular Road Bridge in place and includes the construction of a new structure to the north of the existing bridge. The new structure would be for the new DART tracks and the existing intercity service would continue under the existing South Circular Road Bridge. The new structure requires retaining walls to be constructed on both sides beyond the junction area to the west.

The South Circular Road Junction is extremely busy and frequently has traffic queues, so any works in this area are likely to impact traffic. In order to minimise impact on traffic during the works, the construction will be carried out in phases, utilising all available road space to safely divert all road users around the affected area.

The new structure will accommodate DART trains. This means that the existing South Circular Road Bridge would not need to be electrified and the track levels can be left as they are currently.

The proposed intervention is presented above in sectional elevation looking east towards Heuston Station. In addition, an aerial view of the Emerging Preferred Option for the South Circular Road Junction is also presented above.

26

8.3.7 Heuston Station and Yard

Heuston Station currently does not have any provisions for electrification. Platforms and sidings within the Heuston area are to be electrified to receive the DART+ Fleet. These works will require re-arrangement to provide access to the new DART platforms and to update access to inter-city tracks.

In terms of permanent way works, the constraints on track work in Heuston Station are predominantly those posed by the need to maintain the operational capability.

In the station area, platforms and sidings will be electrified as required for the DART services.

All works can be undertaken within land owned by Iarnród Éireann.

The existing twin tracks along the Phoenix Park Tunnel Branch Line will be electrified. DART+ South West is currently undertaking surveys and analysis along this section, including within the tunnel, to understand the current characteristics and constraints. The Emerging Preferred Option will follow the existing rail corridor and may involve track lowering and/or bridge modifications at certain locations to achieve the height requirements for electrification.

The specific interventions at each bridge along this rail section will be based on the analysis of survey data and presented at Public Consultation No. 2.

8.4 East of South John's Road Bridge to Glasnevin Junction

This area extends from the east of St. John's Road Bridge and northwards over the River Liffey via the Liffey Bridge and under Conyngham Road Overbridge where it enters the Phoenix Park Tunnel.

Close to the junction of the Cabra Road and Navan Road the line exits the Phoenix Park Tunnel and continues north under several road bridges as follows: McKee Barracks Bridge, Blackhorse Avenue Bridge, Old Cabra Road Bridge, Cabra Road Bridge, Fassaugh Road Bridge, Royal Canal and LUAS Twin Arch, the Maynooth Line Twin Arch and Glasnevin Cemetery Road Bridge. The line then continues east and interfaces with the proposed DART+ West Project at Glasnevin Junction.

A requirement of the DART+ South West Project is to investigate the feasibility of a new station at Heuston West, at the site of the existing Platform 10, located to the north west of the greater Heuston Station complex adjacent to the Liffey Bridge. A preliminary assessment for the station has been undertaken by the project team and concept design options are being considered.

The Emerging Preferred Option for Liffey Bridge features electrification and retention of the existing fixed track system.



Northern and Southern Portals to the Phoenix Park Tunnel and view of inside tunnel

27



09

Transport
Integration

9. Transport Integration

The DART+ South West Project will provide high frequency electrified railway transit services running from Hazelhatch & Celbridge Station to Heuston Station, and to Glasnevin via the Phoenix Park Tunnel Branch Line.

The project will link Iarnród Éireann, DART, Dublin Bus, Luas, MetroLink and Dublin Bike services to create a fully integrated public transport system in the Greater Dublin Area.



Public Transport Integration

Public Transport Links

DART+ South West is planned to enhance access and movement of pedestrian and cyclists and reduce reliance on private car. DART+ South West will integrate with the Metropolitan Cycle Network where potential interfaces are identified.

DART+ South West will form a high-capacity spine of a fully integrated public transport system with links to the other public transport modes including Dublin Bus, Bus Éireann, Luas and as well as linking major transport hubs, the project will provide public transport

interchanges at:

- Heuston Station to the Luas Red line; and
- Glasnevin / Phibsborough to the proposed MetroLink and BusConnects schemes.

The project will also secure enhanced passenger access to several of Dublin's higher education institutions as well as enhancing passenger access to other major city attractors such as the Guinness Storehouse.



Increase peak passenger capacity from 5,000 to 20,000 per hour per direction and increase train frequency





10. Issues to Consider

All possible efforts will be made to sensitively address issues and challenges associated with the project at the earliest stages of the design process and public consultation. Several potential issues are detailed below, and the public consultation and stakeholder engagement phases will help us to better understand some of the issues and challenges we face.



Property Acquisition

The DART+ South West Project will predominantly seek to confine the railway improvement works within the existing railway corridor. However, where works and modifications are required outside of the existing corridor, some land acquisition may be required i.e. widening of the railway corridor for four-tracking between Park West & Cherry Orchard Station and Heuston Station and the provision of new electrical substations.

We are now embarking on an extensive programme of consultation with the potentially affected property owners and if your property is likely to be directly affected by the works you should already have heard from us, separately. Our Community Liaison Officer will be available throughout the process to ensure you are regularly updated on the current proposals and your views are taken into consideration as we progress to the 'Preferred Option'.



Environmental Impacts

The project will involve changes to the local environment during both the construction and operation stages, which will result in both temporary and permanent impacts. In order to maintain daytime passenger services during the construction programme night-time works will be necessary. The design process will make every effort to ensure that negative impacts are avoided, reduced or mitigated as far as practicable, and positive impacts are maximised.



Bridge Improvements/Reconstruction

Several bridges on the existing line have been identified as having insufficient height for the electrical equipment as well as width to accommodate four-tracking. Works on these bridges will be needed to accommodate the DART+ South West Project.





Electrification of the Line

Overhead line electrification equipment will be required to provide electrical power to the DART trains. This will involve considerations such as:

- The supply of power from the grid which will need to be agreed and assessed as part of the Railway Order application process.
- The development of substations along the line to provide the power over the proposed circa 20km electrification, with associated land and access requirements.
- The equipment carrying the power lines require certain height clearances from the trains. Several bridges on the existing line have been identified as having insufficient height for this electrical equipment. A range of options are being considered at these locations some of which include modifications to protected structures.
- Boundary walls and fencing along the existing railway may need to be changed. Where necessary wall heights may need to be increased to ensure public safety and the railway is maintained. This is especially relevant due to the erection of overhead electrical lines, to prevent any potential that the public could come in contact with electricity.



11. Next Steps

Further Design Development & Option Selection

The preliminary options selection and design development that has been undertaken has led to the development of the 'Emerging Preferred Option' which is the focus of this public consultation stage.

Once the public consultation process is complete all feedback and submissions received will be reviewed and assessed as part of the next stage of the design development. Following a full appraisal of the feedback, a public consultation report will be prepared to document this process and it will be incorporated into the Options Selection Report.

Further studies, assessments and consultations will lead to development of the 'Preferred Option' which will be presented to the public at Public Consultation later in 2021.

All information gathered by the project team will be used to inform the design development of the project which will be the subject of the Environmental Impact Assessment (EIA) and Appropriate Assessment (AA) (if required), and ultimately the Railway Order application will be submitted to An Bord Pleanála.

The Railway Order Process

The application to An Bord Pleanála for a Railway Order is broadly similar to the planning process which most people are familiar.

The Railway Order application process is set out in the Transport (Railway Infrastructure) Act 2001 (as amended) and the application will be made to An Bord Pleanála. The Environmental Impact Assessment Report (EIAR) will detail the nature and extent of the proposed project and identify and describe the impacts on the environment. It will also detail measures which will be taken to avoid, reduce and/or monitor these impacts. Following the submission of the Railway Order application to An Bord Pleanála, the public are invited through public notices to make submissions which will be duly considered by An Bord Pleanála as part of the decision making process.

We expect that An Bord Pleanála will conduct an oral hearing to provide the public with further participation in the decision making process for this project. At an oral hearing the Iarnród Éireann project team will provide responses to submissions and will be available for questioning. Any person or body may make a submission or observation in writing to the Board in relation to the Railway Order application including the EIAR and the Compulsory Purchase land requirements.

We expect to make the application to An Bord Pleanála in later 2021/ early 2022.



The EIA Process leading to submission of Railway Order to An Bord Pleanála



12. How to Engage

This consultation is our way of asking you, as potential users of the service or those likely to be affected by its development, for your views on our plans. Your local knowledge will inform the emerging design, help us to improve the scheme and ensure it will be a beneficial for you and the communities the route will serve.

The consultation period is now open, full details including closing dates for receipt of submission are available on the project website.

Please contact us via the following means:

Website | www.dartplus.ie

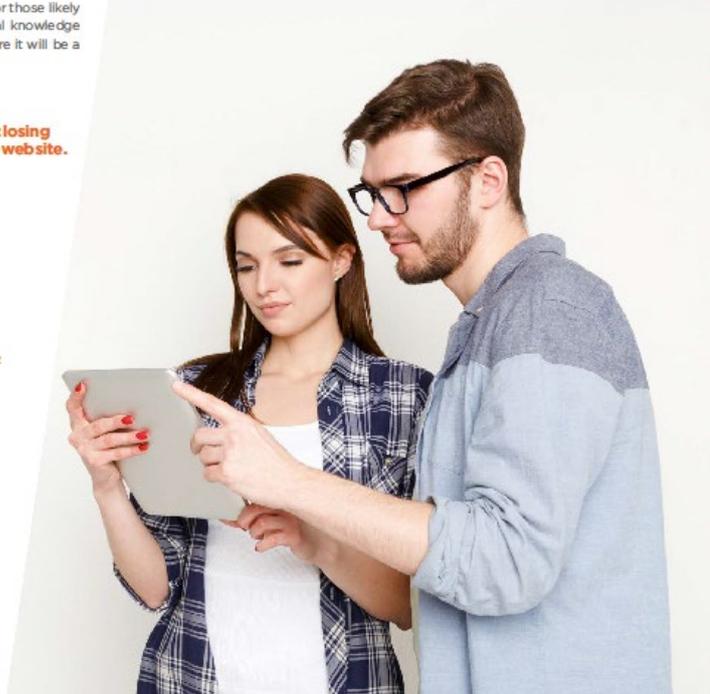
Email | DARTSouthWest@Irishrail.ie

Phone line | (01) 284 1029

Postal Address

If you would prefer to write to us, please send it or any correspondence to:

Community Liaison Officer,
 DART+ South West,
 Iarnród Éireann,
 Inchicore Works,
 Inchicore Parade,
 Dublin 8.
 D08K6Y3



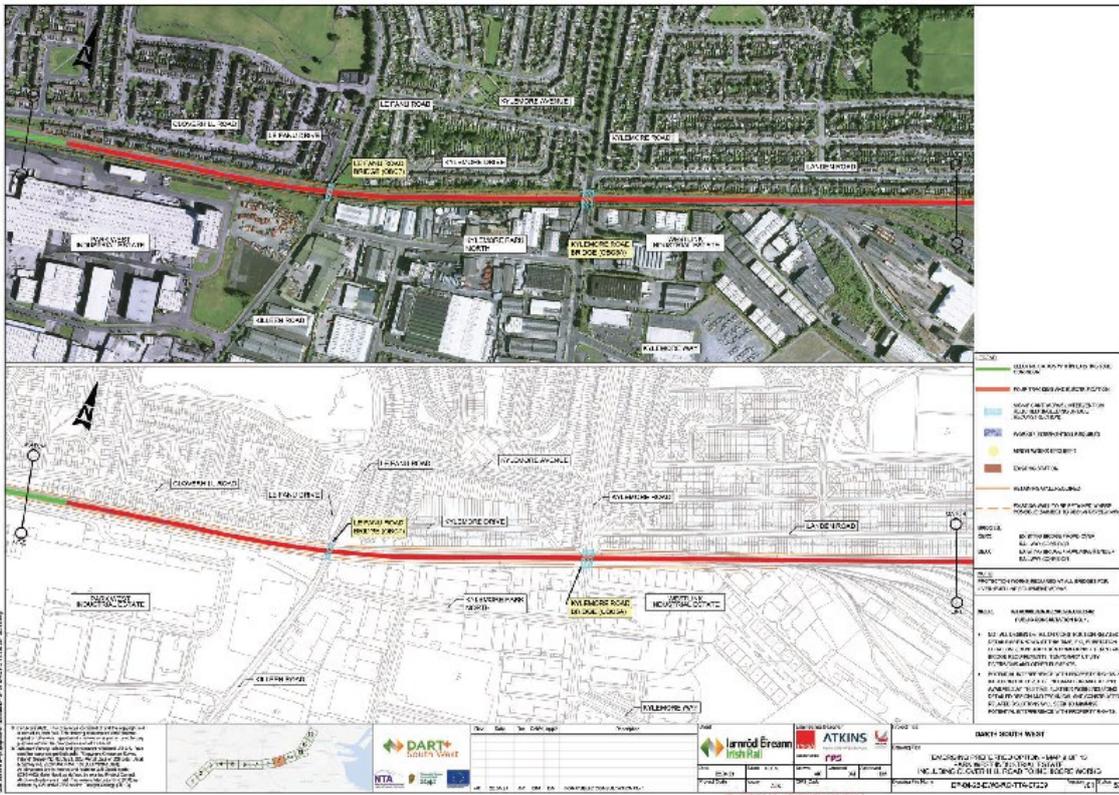
36

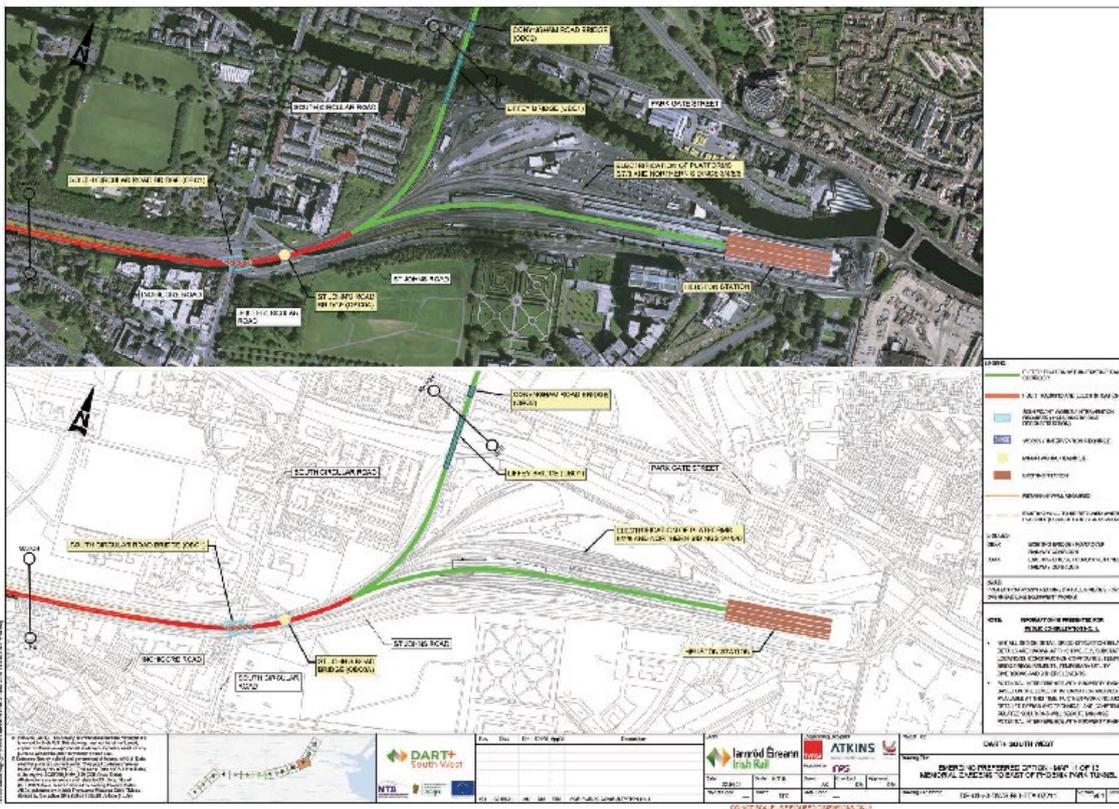
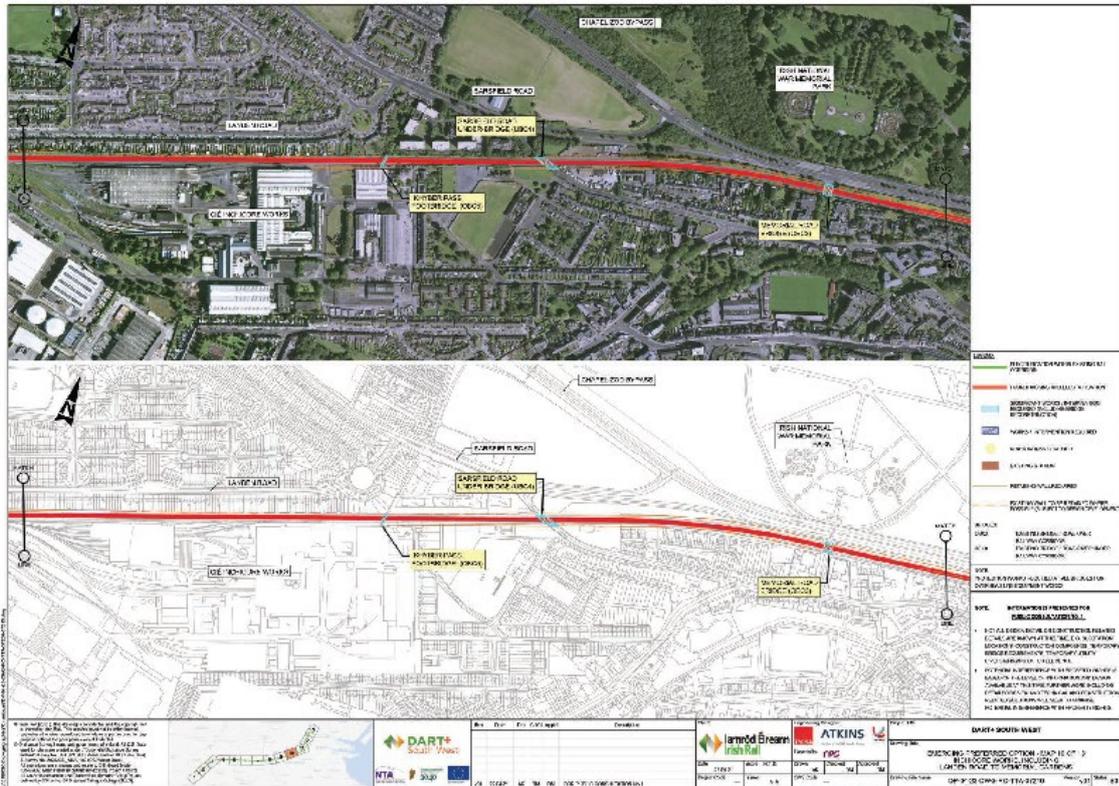


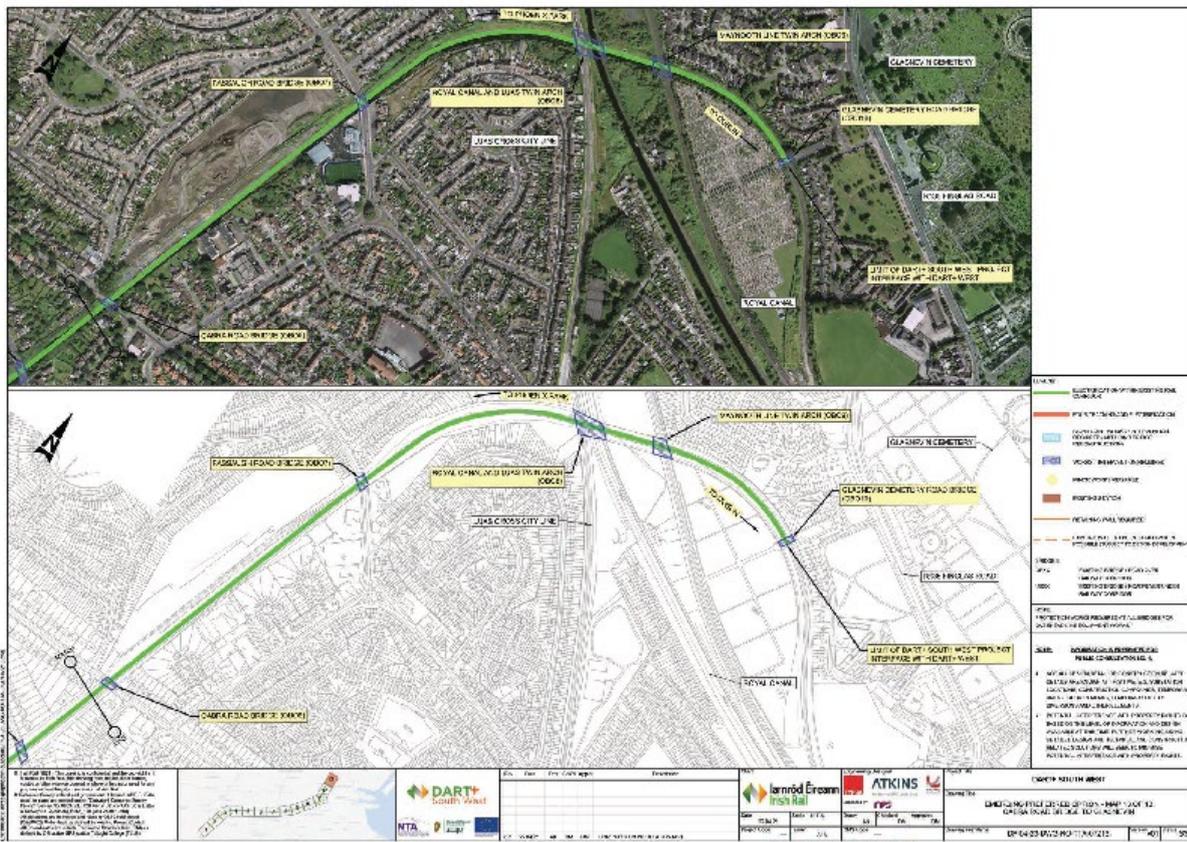
13

Alignment
Figures









DART+
South West



Appendix D. Website Screenshots and Frequently Asked Questions

Website Screenshots

Welcome to the DART+ South West Project

The second of the infrastructural projects of the DART+ Programme to be delivered will be the DART+ South West Project. This rail improvement project will provide a sustainable, electrified, reliable and more frequent rail service to our customers, revolutionising capacity between Hazelhatch & Celbridge station and Dublin City Centre, whilst also increasing capacity and reducing journey times for non-electrified InterCity and Commuter services.



DART+ South West Virtual Exhibition Room

Iarnród Éireann wants to provide you with every possible opportunity to learn about the DART+ South West Project and get involved in the design development process. A virtual exhibition space



- [Key Infrastructural Elements](#)
- [DART+ South West Route Map](#)
- [Benefits of DART+ South West](#)
- [DART+ South West Emerging Preferred Option](#)
- [DART+ South West Public Consultation Process](#)**
- [DART+ South West Useful Material and Downloads](#)
- [DART+ South West Next Steps](#)
- [How to Engage/Contact Us](#)
- [Frequently Asked Questions](#)

DART+ South West Public Consultation Process

Public participation is a key element to the delivery of major infrastructure projects, such as the DART+ South West Project. There are three main opportunities for customers, neighbours and interested parties to provide feedback to the scheme development and Railway Order approval process as outlined below:

- Public Consultation No.1 on the Emerging Preferred Option (Spring 2021) - Current Stage
- Public Consultation No. 2 on the Preferred Option (Summer 2021)
- Statutory Consultation Period as part of the Railway Order Application process (Winter 2021/Spring 2022)

These are highlighted in graphical form below:



Frequently Asked Questions

1. My house backs on to the existing railway line. What will the impact be?

Construction Phase

The starting principle for the Project is to upgrade the existing railway corridor and to undertake all works, within the railway corridor. This can be achieved over the majority of the route, including building on the groundwork carried out under the original Kildare Route Project, which delivered the existing four track system and several reconstructed bridges from Hazelhatch & Celbridge Station to Park West & Cherry Orchard Station. The last remaining significant constraint is the area between Park West and Heuston Station, where four tracks reduce to two tracks. Extending to four tracks in this area will require an increase in the width of the existing rail corridor and this will have a potential impact on adjoining property owners.

In order to minimise construction impacts, the majority of works will be carried out within the existing rail corridor, where possible. In order to maintain services during the day, the majority of the construction works along the railway line itself will take place at night. Works outside of the live railway corridor can progress during the day (i.e. construction of bridges associated with bridge widening, substations, construction compounds). Every effort will be made to avoid, reduce, and/or mitigate negative impacts, however, there is likely to be some disturbance experienced by those in close proximity to the railway line caused by noise, lighting or fencing/hoarding erected associated with the construction activities. The types of construction work required at each specific location will determine the type of impact that may affect the area of your property. However, there will be general linear works required along the full length of the route, such as:

- Overhead electrification equipment along the full extent of the railway line. This will be similar in style to that currently used on the existing DART network.
- Modifications to the existing rail bridges and tunnels, such as modifications to the structure, track lowering or a combination of both.
- Substations will be required at intervals along the rail line to provide power to the network.
- Signalling upgrades and additional signalling will be required to the upgraded infrastructure.

Interfaces with existing utilities, boundary treatments, drainage works, vegetation management and other ancillary works will be required along the length of the project. Upon appointment of a construction contractor a dedicated Community Liaison Officer will be put in place to communicate details of upcoming works and every potential mitigation will be put in place to minimise the disruption that may occur.

Operational Phase

During the operational phase, the frequency of service will increase.

Assessment of Impacts

All likely significant effects during both the construction and operational phases will be identified and detailed in the Environmental Impact Assessment Report with a detailed schedule of mitigation measures identified to reduce those potential effects.

2. How close will the new tracks be to my house/back wall?

Track layout will remain unaltered between Hazelhatch & Celbridge Station and the Park West & Cherry Orchard Station and no trains will be closer than at present to property boundaries in between these stations. Between Park West & Cherry Orchard Station to Heuston Station widening of the railway corridor and completion of four tracking will potentially move some tracks closer to properties boundaries. For this section, design development is currently in progress and specific property boundaries that may be moving close are yet to be finalised. If your property has been identified as potentially impacted by the proposals, a letter will have been delivered to your property. A dedicated landowner specialist will be available to meet with individual property owners and

provide regular updates on the project. In addition, a community liaison office will also be available to provide regular updates on the project.

If you have a query, please contact the project team. Our contact details are available [here](#).

3. Will my house/wall be damaged?

The works will be undertaken in a manner so as to avoid impacts on adjoining properties. Subject to Railway Order approval and as necessary, a condition survey will be provided of existing structures and buildings immediately adjacent to the works at certain locations. These will be determined on a case by case basis depending on the works required at that location. Should your property be deemed to require a condition survey you will be notified, and your permission sought to conduct the survey. The condition survey would take place at the preconstruction stage to provide assurance to property owners.

4. Will there be improved cycling facilities at the existing stations?

The DART+ South West Project will replace or enhance (where practicable) pedestrian and cycle facilities where bridge reconstruction is necessary. Pedestrian and cycle facilities associated with many of the existing stations were provided as part of the original Kildare Route Project; the facilities are consistently under review and are the remit of the IE Station Enhancement Programme.

5. Is the project compatible with a future LUAS?

The Kylemore Road Bridge replacement proposals for DART+ South West will be compatible with the future provision of LUAS across Kylemore Road Bridge in terms of accommodating the necessary loading and providing the flexibility to extend the bridge laterally in the future.

6. Will DART+ link with the future DART Underground Project?

Alignment proposals for DART+ South West will be compatible with the future provision of DART Underground.

7. What is the timeline for commencement of service of the new electrical vehicles?

The DART+ South West project aims to commence commercial service in 2026, further to completion of construction works, testing and commissioning, which are subject to availability of funding.

8. How does the Railway Order process work?

Railway Order application is broadly similar to the planning application process. The project is categorised as Strategic Infrastructure Development (SID) and Iarród Éireann applies directly to An Bord Pleanála for permission. The Railway Order application process is set out in the Transport (Railway Infrastructure) Act 2001 as amended by the Strategic Infrastructure Act 2006. Following two phases of public consultation, we will submit the Railway Order application. Any person or body may make a submission or observation in writing to An Bord Pleanála in relation to the application and / or the Environmental Impact Assessment Report and / or the compulsory acquisition requirements.

The Railway Order application will include a number of technical documents and project drawings and an Environmental Impact Assessment Report. All of these documents and drawings together with any feedback/submissions received from the public as part of the statutory public consultation process will be reviewed and considered by An Bord Pleanála before a decision on the application is made. We expect that An Bord Pleanála will conduct an Oral Hearing before they make a decision. At an Oral Hearing the authors of relevant reports and experts will give evidence on the submissions received and will be available for questioning. Further information on making a submission / observation in writing to the Board and Oral Hearing procedures are available from the Board's website.

9. Will there be an Environmental Impact Assessment Report (EIAR)?

Yes, an EIAR will be prepared for the project which will contain detailed analysis of the potential impacts of the proposed project on the existing environment and will include sufficient information to allow the consenting authority, in this case, An Bord Pleanála, to decide on whether consent should be given to the project.

The EIAR will present a description of the existing environment, an assessment of the potential impacts of the scheme, will set out measures to avoid or reduce any adverse impacts and will identify any remaining residual effects. The impacts will be assessed and presented in line with the environmental topics, and in accordance with Transport (Railway Infrastructure) Act 2001 as amended and the EIA Directive 2014/52/EU.

10. Will any services to stations stop/be affected by the project?

The project will continue to serve all the existing stations between Hazelhatch & Celbridge Station and Dublin City. The only other change will be that there will be more train services and more passenger capacity on these services.

11. Why do you need to widen the rail corridor at Inchicore as there are already four tracks?

The railway along this section comprises two main line tracks which are joined by two sidings used to access the depot and for train storage. The Project requires two additional operational DART tracks as well as the existing tracks to facilitate the required increase in capacity.

12. Will access to private 3rd party land be required?

Some access to third party lands will be required. Should this be required formal contact will be made with the relevant landowner and permission will be sought for access.

13. How will the local community benefit?

DART+ South West Programme is seeking to significantly increase the frequency and capacity of train services between Hazelhatch & Celbridge & Heuston/Grand Canal Dock. This can be achieved by changing to electrified, high capacity DART trains and increasing the frequency of trains. Delivery of this project will support the existing communities along the railway and support future sustainable development. It will serve all existing stations along the route as well as Kishoge Station in the future, using electrical power that has a lower carbon footprint than the existing diesel trains. The frequency and quality of service that will be provided will provide a viable transport alternative to communities along the route and help encourage people to switch from private car use. This will assist in Ireland reducing greenhouse gas emissions from transport and help combat climate change. The electrification of the rail line will predominantly follow the existing railway corridor.

14. Is the project needed in light of COVID-19?

Despite the impacts of COVID-19 on public transport and passenger numbers as a whole, Iarnród Éireann, and the National Transport Authority remain firmly committed to the DART+ South West Project and supporting the implementation of Project Ireland 2040 and the National Planning Framework. DART+ South West is required to secure the long-term sustainability of public transport post-Covid life in the Greater Dublin Area and will ensure that Ireland meets its many ambitious long-term national climate change targets and in transportation going forward.

15. I am a regular commuter between Hazelhatch & Celbridge and Grand Canal Dock; will I have direct services?

Yes. The DART+ South West will provide capacity for up to 11 services per hour and per direction. Four will finish service at Heuston and seven will follow the Phoenix Park Tunnel Branch Line towards the Docklands area.

16. I regularly travel on the Cork Mainline, what will DART+ South West do for me?

Passengers who regularly travel on the Cork Mainline train service will benefit from more frequent and reliable service when the project is finished. There is currently a maximum of 12 trains per hour in each direction. After DART+ South West is completed, services will increase subject to demand to 23 trains per hour per direction (i.e. maintain the existing 12 services, with an additional 11 train services provided by DART+ South West).

Also, the type of train you will be travelling on will be different. They will be a DART type electric or battery-electric train. These trains are more environmentally friendly than the current diesel-powered trains which will contribute to reducing greenhouse gas emissions from the transport sector and supports the Governments Climate Action Plan.

The utilisation of DART trains as far as Hazelhatch & Celbridge will increase the passenger carrying capacity from approximately 5,000 to approximately 20,000 passenger per hour per direction. The project will link good quality public transport to sustainable land use management and can also assist in local regeneration, economic development and support the development of new communities along the route. This is a key objective of Project Ireland 2040 and the National Planning Framework. The integration of public transport with sustainable land use planning will reduce the dependency on private car use and ultimately support reductions in greenhouse gas emissions from the transport sector.

DART+ South West will integrate with other public transport modes (other DART+ projects, Bus, LUAS and proposed MetroLink) as well as walking and cycling infrastructure. This will have a positive effect on transport patterns and lifestyle choices. The provision of sustainable transport network supports options for where people live, work, study, access services and use public amenities. It can promote more active and healthy modes of travel by supporting people to walk or cycle to public transport links for onward transfer to their end destinations.

Appendix E. Virtual Consultation Room Panels



PUBLIC CONSULTATION NO. 1

Emerging Preferred Option
12 May 2021 - 23 June 2021



PUBLIC CONSULTATION NO. 1
 Emerging Preferred Option



What is DART+ South West

The DART+ South West Project, as part of the DART+ Programme, will deliver an improved electrified network, with increased passenger capacity and enhanced train service between Hazelhatch & Celbridge Station and Heuston Station (c. 16km) on the Cork Mainline, and to Glasnevin via the Phoenix Park Tunnel Branch Line (c. 4km).



DART+ South West will significantly increase train capacity from the current 12 trains per hour per direction to 23 trains per hour per direction (i.e. maintain the existing 12 services, with an additional 11 train services provided by DART+ South West).

This will increase passenger capacity from the current peak capacity of approximately 5,000 passengers per hour per direction to approximately 20,000 passengers per hour per direction, as the new fleet being delivered as part of the DART+ Programme will have greater carrying capacity.

Delivery of DART+ South West will support existing communities and support future sustainable development. It will serve all existing stations along the railway corridor between Hazelhatch & Celbridge Station and Dublin City Centre using electrical power, which has a lower carbon footprint than the current diesel trains. The frequency and quality of service will provide a viable transport alternative for surrounding communities other than private car travel. This will assist in Ireland reducing road congestion and greenhouse gas emissions from transport, thereby helping to combat climate change.



PUBLIC CONSULTATION NO. 1
 Emerging Preferred Option



Benefits of DART+ South West



Increase peak passenger capacity from 5,000 to 20,000 per hour per direction and increase train frequency between Hazelhatch & Celbridge Station and Dublin City – facilitating fast, frequent and reliable transport to the surrounding communities.



Enhance public transport opportunities for work, education or leisure purposes.



Facilitate the development and future growth of existing and new communities that will greatly benefit from the connectivity that the DART+ South West will deliver.



Alleviate road congestion.



Build a sustainable and connected city region, supporting the transition to a low carbon and climate resilient society.



Facilitate people to make sustainable travel choices by encouraging a move away from private cars to reliable, efficient and safe public transport network.



Improve multimodal transport connectivity through interchange with the Luas at Heuston Station, Bus Connects and the proposed MetroLink.



Improve journey time reliability.

PUBLIC CONSULTATION NO. 1
Emerging Preferred Option




DART+ South West Map



PUBLIC CONSULTATION NO. 1
 Emerging Preferred Option



Key Infrastructural Elements

The key infrastructural elements of the DART+ South West Project includes:

- Completion of four tracking, widening the rail corridor, from Park West & Cherry Orchard Station to Heuston Station, extending the works completed on the route in 2009.
- Electrification and re-signalling of the line from Hazelhatch & Celbridge Station to Heuston Station and also from Heuston Station to Glasnevin, via the Phoenix Park Tunnel Branch Line, where it will link with the proposed DART+ West.
- Undertaking improvements/reconstructions of bridges to facilitate movement of electrified train services.
- Remove rail constraints along the Phoenix Park Tunnel Branch Line.
- Feasibility report and concept design for a potential new Heuston West Station.

The **'Emerging Preferred Option'** will be compatible with future stations at Kylemore and Cabra, although the construction of these stations is not part of the DART+ South West Project.





Emerging Preferred Option

The DART+ South West Project includes general linear works between Hazelhatch & Celbridge Station and Glasnevin Junction to enable the electrification of the railway line, as well as specific interventions at particular areas along the project corridor.

The starting principle for the project is, where possible, to undertake all improvement works within the existing railway corridor to minimise impacts on adjoining property. For the purposes of describing the Emerging Preferred Option, the project corridor is divided into the following three sections:

Hazelhatch & Celbridge Station to Park West & Cherry Orchard Station

The Emerging Preferred Option for this section of the project corridor builds on work carried out under the original Kildare Route Project, which delivered the existing four track system and several reconstructed bridges between Hazelhatch & Celbridge Station and Park West & Cherry Orchard Station. In this regard, works along this circa 11km section comprises of general linear works only.

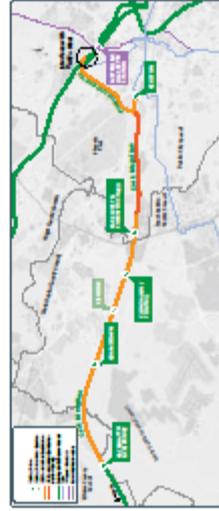
Park West & Cherry Orchard Station to Heuston Station

The Emerging Preferred Option for this circa 5km section of the project corridor comprises of general linear works as well as the provision of four tracks and alterations to structures as necessary.

- Le Fanu Bridge;
- Kylemore Road Bridge;
- Inchicore Works;
- Sarsfield Road Bridge;
- Memorial Road Bridge;
- South Circular Road Junction; and
- Heuston Station and Yard.

East of St John's Road Bridge to Glasnevin Junction

The Emerging Preferred Option for this circa 4km section comprises of general linear works and track lowering / bridge modifications as necessary.



PUBLIC CONSULTATION NO. 1
Emerging Preferred Option



Public Consultation Process

Public participation during the design process is a key element to the delivery of major infrastructure projects such as DART+ South West.

Public Consultations are our way of asking you, as potential users of the improved services or those likely to be affected by its development, for your views on our plans, whilst the design process is active. Your local knowledge and comments will inform the design and help us improve the project, ensuring it will be a success for you and the communities it will serve.

This project has two non-statutory Public Consultations and one Statutory Consultation Period as part of the Railway Order application process.

This current consultation is Public Consultation No. 1 seeking feedback on the **'Emerging Preferred Option'**.

- Public Consultation No. 1 on the Emerging Preferred Option (Spring 2021) - **Current stage**
- Public Consultation No. 2 on the Preferred Option (Summer 2021)
- Statutory Consultation Period as part of the Railway Order application process (Winter 2021/Spring 2022)

Iarnród Éireann invites you to engage in the design process and all feedback is welcome. Your feedback will be accepted during all stages of the design development and can be submitted through the project website, e-mail address, phone line or by written correspondence.

For further details on how to submit your feedback please see the **'How to Engage'** section or visit our website www.dartplus.ie.

COVID 19 Due to COVID-19 restrictions the first consultation on the **"Emerging Preferred Option"** will be a predominantly digital online public consultation. If COVID-19 restrictions ease further into 2021, the second public consultation event, scheduled for the Summer of 2021, presenting the **"Preferred Option"** to the public may be possible to hold in a physical location.

PUBLIC CONSULTATION NO. 1
 Emerging Preferred Option



Next Steps

Feedback received during this public consultation will be considered and will inform the development of the **'Preferred Option'** which will be presented at a later **Public Consultation event** scheduled for later in 2021.

The options selection and early stage design development that has been undertaken has led to the development of the **'Emerging Preferred Option'** which is the focus of this public consultation stage.

When this public consultation process on the **'Emerging Preferred Option'** closes, all feedback and submissions received will be reviewed and assessed as part of the next stage of the design development. Following a full appraisal of the feedback, a public consultation report will be prepared to document this process and it will be included in an Emerging Preferred Route Public Consultation Findings Report (*to be presented at Public Consultation No. 2*).

The feedback and inputs gathered from the public during this consultation process, along with further assessment and design development, will inform the project development process allowing for the **'Emerging Preferred Option'** to be refined and the **'Preferred Option'** to be determined.

All information gathered by the project team will be used to inform the design development of the project which will be the subject of the Environmental Impact Assessment Report (EIAR) and Appropriate Assessment (AA) (if required), and ultimately the Railway Order application will be submitted to An Bord Pleanála.



The EIA Process leading to submission of Railway Order to An Bord Pleanála

PUBLIC CONSULTATION NO. 1
Emerging Preferred Option



How to Engage

The project team would like to hear your views on the 'Emerging Preferred Option' to inform us in the development of the project.

We welcome your feedback on any issues or information, related to the project, which you think should be considered by the project team in progressing the 'Preferred Option'.

All project information, including maps and reports are available to view on the project website.

The DART+ South West Project team are available to answer any of your questions and are available to assist you regarding the project via the following means:

Website | www.dartplus.ie

Email | DARTSouthWest@irishrail.ie

Phone line | (01) 284 1029

Postal Address

If you would prefer to write to us, please send it or any correspondence to:

Community Liaison Officer,
DART+ South West,
Iarnród Éireann,
Inchicore Works,
Inchicore Parade,
Dublin 8.
D08K6Y3

All feedback for Public Consultation No. 1 should be returned to the project team by the **23rd June 2021**

